

TOWN OF RICHMOND HILL
PLANNING AND DEVELOPMENT
DEPARTMENT

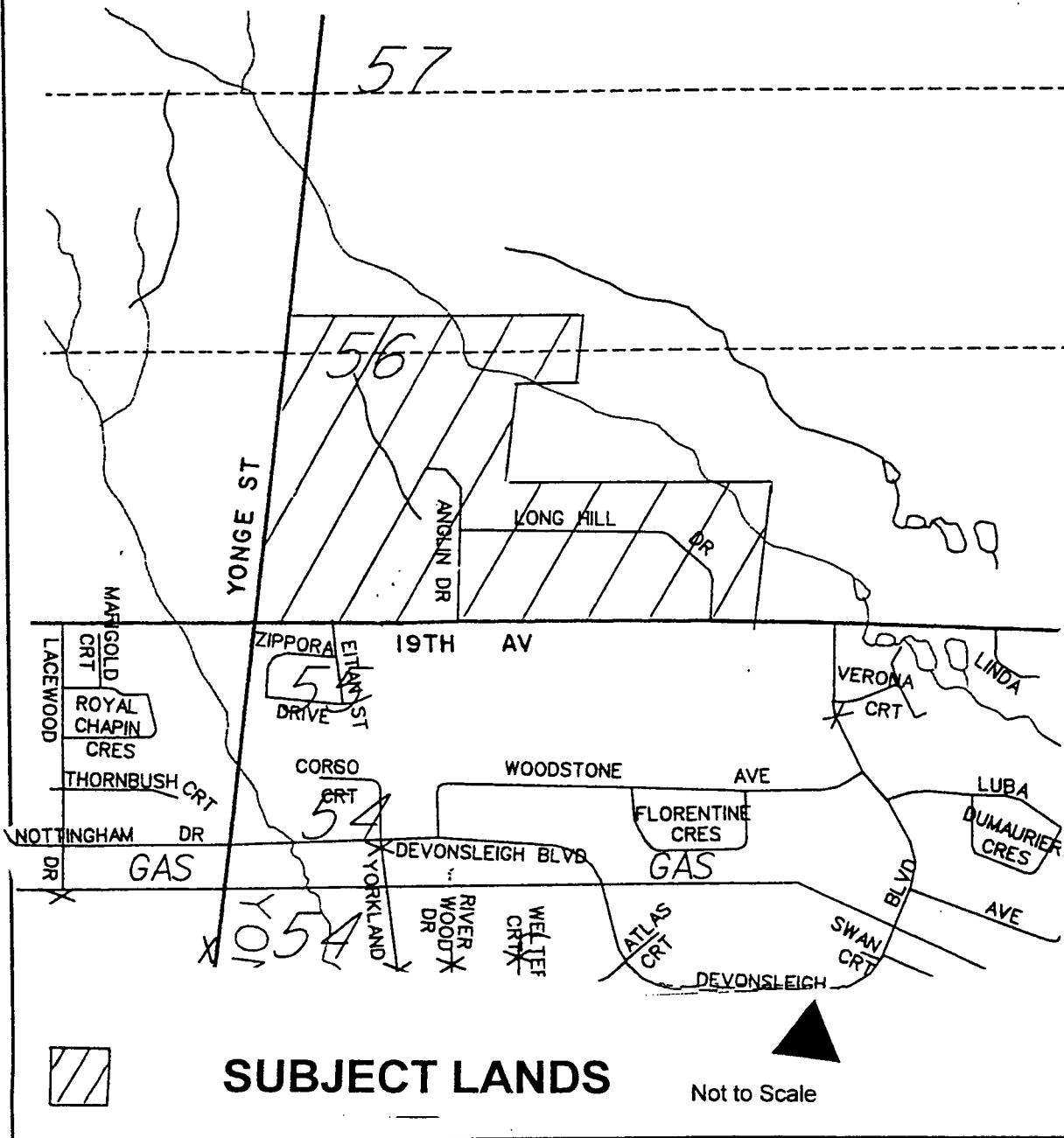
COUNCIL MEETING

PREPARED BY: Town Staff
TELEPHONE NO.: 905-771-8910

JANUARY 20, 1999
STAFF REPORT SRP.99.003
FILE NO.: D00-UF-LA

Anglin Drive/Long Hill Drive/19th Avenue Infill Study

Location Map



SUBJECT

Residential Infill Study for the Anglin Drive/Long Hill Drive/19th Avenue Neighbourhood.

RECOMMENDATIONS:

1. That Council receive Staff Report SRP.99.003 and approve the following key principles, prepared by The Planning Partnership, to guide future infill development in the Anglin Drive/Long Hill Drive/19th Avenue neighbourhood:
 - a) new development should be designed to blend with existing conditions, including separation between units, extensive trees and other vegetation and deep setbacks from the streets to the houses;
 - b) minimum disruption to areas with steep slopes and natural features;
 - c) protection against noise from 19th Avenue and Yonge Street; and
 - d) minimize direct access to 19th Avenue.

2. That the Neighbourhood Infill Study prepared by The Planning Partnership and contained in Appendix "A" to Staff Report SRP. 99.003 be endorsed and used to guide the detailed planning and design of infill development in the Anglin Drive/ Long Hill Drive/19th Avenue neighbourhood and the evaluation of future development applications.

Background

On October 15, 1997, Official Plan Amendment No. 138 (O.P.A. 138) was approved by the Region of York. Within the "Residential Policies" Section of O.P.A. 138, Section 4.2.1 states the following:

"Infilling and redevelopment of Registered Plan 5509, 65M-2075 and abutting lands, generally known as the Long Hill Drive/Anglin Drive neighbourhood, shall be permitted subject to the policies of Section 2.2.1.4.8 of the Official Plan. Infilling and redevelopment, and any implementing Zoning By-law or development application shall not be considered by Council until full municipal services are available and a comprehensive concept plan for infill has been prepared to the satisfaction of the Town."

Section 2.2.1.4.8 of the Official Plan is the "Infilling" Section of the Official Plan and includes general policies for infill developments throughout the Town of Richmond Hill.

In satisfaction of the second component of the above section, the Town retained The Planning Partnership to prepare concept plans, design guidelines, and a draft zoning by-law for the Anglin/Long Hill/19th neighbourhood. On March 9, 1998, the Town held the first of three neighbourhood meetings to introduce the Town's consultants and discuss the study approach.

Following the meeting, a questionnaire was distributed to the area residents in May. In July, the Town held the second neighbourhood meeting to review the results of the questionnaire and illustrate several potential redevelopment scenarios in the form of concept plans. The third neighbourhood meeting was held in November of 1998 and was primarily focused around the servicing aspects of infilling and redevelopment, including options for construction, timing, and costs. The neighbourhood was informed at this time that Staff would be proceeding to Council early in the new year.

It should also be noted that an application for zoning by-law amendment and draft plan of subdivision was submitted in October, 1998 for the lands at the north-east corner of Yonge Street and 19th Avenue (see Map 2). The application proposes street townhouses on the western half of the lands and single detached wide shallow lots on the east half of the lands. The applications are required to be heard at a Public Meeting of Council, however, the applicant has been informed in writing of several concerns from various departments and agencies including intersection alignment, park location, preservation of the existing landform, size and design of lots, Yonge Street streetscape, and making provision for backlotting for the Anglin Drive residents. The applicant has responded that they are committed to continuing the process of design and development of the plan on an interactive basis with the Town.

ANALYSIS

The Anglin and Long Hill Drive Study

Attached as Appendix "A" is the Anglin and Long Hill Drive Neighbourhood Infill Study prepared by The Planning Partnership. The report includes the following components:

- introduction to the concept of infilling
- results of the above noted questionnaire
- design principles
- infill and redevelopment scenarios
- proposed zoning standards

It is important to note that Staff is **not** recommending that Council pick one of the infill and redevelopment concept plans shown in the study as the definitive and absolute plan by which development shall proceed. Instead, the concept plans are intended to be used for various purposes including initiating discussion amongst the residents, indicating possible illustrations for achieving redevelopment in accordance with the key principles identified in the report, and generally guiding the detailed planning and design of infill development in the Anglin Drive/ Long Hill Drive/19th Avenue neighbourhood and the evaluation of future development applications.

The Development Process

The following provides some general information about the development process in the Town, including some preliminary cost estimates for the creation of lots.

Zoning

The current zoning of the majority of the study area is Rural Residential One (RR1) under Zoning By-law No. 2325-68. In order to facilitate the splitting or "severance" of an existing residential lot, the RR1 zoning must be amended and it is recommended that the R6 Zone be implemented in the area, with a 7.5 metre front yard setback, instead of the normal 6.0 metre setback. Some of the development standards of the RR1 Zone and the proposed R6 Zone are listed below.:

	RR1	R6
Minimum lot area	0.4 ha (1 acre)	500 square metres (5382 sq. ft.)
Minimum lot frontage	45 metres (150 feet)	15.0 metres (49.2 ft)
Minimum front yard	7.6 metres (25 feet)	7.6 metres (25 ft)
Minimum interior side yard	3.0 metres (10 feet)	1.5 metres (4.92 ft)
Minimum rear yard	7.6 metres (25 feet)	7.5 metres (24.6 ft)

The R6 zoning could be implemented on a site specific basis or, if consensus can be obtained within the study area, one overall area by-law amendment for R6 zoning could be forwarded to Council. Currently, there are several residents who wish to maintain their properties in the current state and therefore, do not support an overall amendment to the zoning by-law.

With respect to site specific zoning, if one resident and/or a few abutting neighbours were planning to sever their lands, a zoning amendment application could be filed by the owners for their lands only. A public meeting of Council would then be held and Council would determine if the application should be approved. It should be noted that due to the substantial amount of relief that would be required from the provisions of the by-law in order to create a 50 ft lot and build a house on the lot, Staff will not support reductions to the new standards by way of minor variance applications.

It is also important to note that, given the large widths of the primarily bungalow houses in the area, the creation of any more than one or two lots from an existing lot will most likely require the demolition of the existing house.

Consents/Subdivisions

Once the zoning is in place, there are two types of applications that can be used to sever land into new lots: consent or draft plan of subdivision. In Richmond Hill, consent applications are processed through the Committee of Adjustment and draft plans of subdivision are processed through Council.

Consent applications are used when the number of new lots to be created is less than three (3) and when there is no extension, improvement or assumption of a public road, a new road allowance, or municipal services. This means that no consent applications will be recommended for approval until the subject lands are appropriately zoned and all of the required services have been installed.

The other method of lot creation is through a draft plan of subdivision. Subdivision plans are generally required where the number of new lots is three (3) or more or when a new road is being proposed. For example, if a number of owners get together, they can hire a consultant to draft a plan and submit it to the Town.

Once accepted by the Town as a complete application under the requirements of the Planning Act, a subdivision plan is then circulated to various departments and agencies for comments. Depending on the nature of the comments received, a report that reviews the application and makes recommendations is then heard at a Public Meeting of Council. If the outcome is positive, the application may be given "draft approval" by Council at the next available Council meeting. Draft approval means that the subdivision is approved, subject to several conditions. Once the conditions are fulfilled, the plan may be registered and the new lots then become legal entities. At this stage, one can make application for building permits on the new lots.

Servicing

As noted above, before any infilling is approved, municipal services must be available to the area. Municipal services in this area consist of municipal water and sanitary sewer. The water will most likely come across 19th Avenue from Yonge Street (Map 3) and the sanitary sewer can be extended to the area from Devonsleigh Blvd. to the east (Map 4). There is another option identified on Map 4 as an "Alternative Sewer Connection" through the proposed subdivision to the south of the study area. Although this may be a more cost effective method, the timing of the construction of this subdivision lies with the owner of these lands.

With respect to how the services are extended to the neighbourhood, there are essentially three options as follows:

1. The services could be installed by the Town through "local improvements". However, it should be noted that Anglin Drive and Long Hill Drive are currently in good condition and therefore, local improvements are not contemplated to occur for several years.
2. The services could be installed by a developer, such as Crossmar, and development charges to cover the costs of installation will be applicable to each new lot created in the neighbourhood. This is generally a more expensive option than the following option.
3. The services could be installed by a developer in cooperation with the local residents. Residents who are interested in developing their lands could enter into an agreement with a developer to install the services. This option should be the most cost effective method of service installation for all involved parties.

Costs

In addition to the costs to install the services noted above, there are other charges that are payable during the severance or subdivision process. The following Table 1 indicates the primary costs to create a 50 ft parcel of land, including an estimate of the servicing costs. The table does not include fees such as application fees, consultant's fees, surveyor's fees, and lawyer's fees.

An issue has been raised with respect to the parkland component of the following Table. The issue is that since parkland was provided through the original plan of subdivision that created the existing lots, there should be no need for a cash-in-lieu of parkland charge. Staff is currently reviewing the concern.

Table 1

January, 1999

**EXAMPLE APPROXIMATE COSTS
TO CREATE ONE 50 ft LOT
(Example with Cost Sharing and Local Improvements)**

<u>TYPE OF COST</u>	<u>COST</u>
Cash in Lieu-of-Parkland: Payable at building permit	\$3,200 per frontage ft. (5%) eg. 50 ft. x 3200 x 5% = \$8,000
Educational Development Charge Payable at building permit Separate School Board <u>Public School Board</u> Total Educational DC	\$ 730.00 per unit <u>\$1,620.00 per unit</u> \$2,350.00 per unit
RH Hydro Development Charge Payable at subdivision agreement <u>Payable at building permit</u> Total RH Hydro DC	\$482.40 per unit <u>\$321.60 per unit</u> \$804.00 per unit (for consent, \$804.00 is payable at building permit)
Region of York Development Charge Payable at subdivision agreement: Water San Sewer Roads Total Payable at building permit: general services Total Region of York DC	\$2,890.00 per unit \$2,443.00 per unit <u>\$3,064.00 per unit</u> \$8,397.00 per unit <u>\$723.00 per unit</u> \$9,120.00 per unit (for consent, \$9,120.00 is payable at building permit)
Town of Richmond Hill DC Payable at building permit:	\$7,122.00 per unit
Servicing Costs Watermain Sanitary Sewer Road Restoration	Cost Sharing/Front Ending OR Local Improvements \$ 23.5 x 50 = 1,175 43 x 50 = 2,150 \$ 59.5 x 50 = 2,975 56 x 50 = 2,800 <u>\$ 52 x 50 = 2,600</u> <u>62 x 50 = 3,100</u> Total \$ 6,750 \$ 8,050
TOTAL COST (Parks Cash-in-Lieu, Development Charges, & Servicing)	\$ 34,146.00 OR \$ 35,446.00

Please note that these charges are subject to indexing pursuant to the various by-laws in place by the Town. Confirmation of current charges should be obtained from the Town's Finance Department.

The above servicing costs represent an estimate based upon current construction values and engineering standards and are subject to change.

Resident's Responses

A draft of the report attached as "Appendix A" was delivered to the houses within the infill area and comments were requested. As of the writing of this report, several letters have been received and they are attached in "Appendix B".

Conclusions

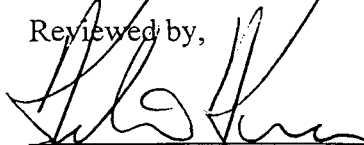
Official Plan Amendment No. 138 allows for the infilling and redevelopment of the subject area. In order to ensure that the infilling and redevelopment proceeds in the most appropriate fashion possible, OPA No. 138 requires a comprehensive concept plan for infill be prepared and also requires new development to be on full municipal services. The Town retained the Planning Partnership to prepare a study of the area. Throughout the last 10 months, the Town and its consultants have canvassed the neighbourhood and several neighbourhood meetings were held.

It is evident that many residents in the neighbourhood have a desire to develop their lands, while others currently do not wish to develop and would like to continue living in their existing homes. The attached report addresses the resulting issues and presents some possible redevelopment scenarios.

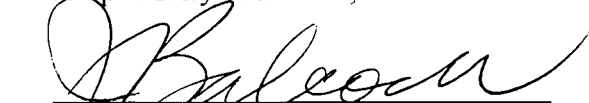
For those that wish to develop their lands, their options include proceeding by way of severance or subdivision application. However, prior to any severance or subdivision approval, the proponents must have their lands rezoned and must have access to full municipal services. There are costs associated with development, including bringing the services to the neighbourhood, development charges, application fees, consultants fees, and others.

This Staff report and the attached report by the Planning Partnership are intended to help the neighbourhood deal with the increasing pressures of development and its also intended to assist the Town to guide the detailed planning and design of infill development in the Anglin Drive/ Long Hill Drive/19th Avenue neighbourhood and the evaluation of future development applications.

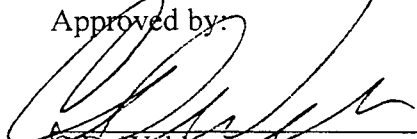
Reviewed by,


Kelvin Kwan, M.C.I.P., R.P.P.
Manager of Development

Respectfully submitted,


Janet E. Babcock, M.Sc., M.C.I.P.
Commissioner of Planning and Development

Approved by:


C.B. Weldon,
Chief Administrative Officer

DATE _____, 1999
MM:KK:JEB:CDW:mm

MM MM

APPENDIX CONTENTS

“A” Infill Report

“B” Correspondence

- B1. Letter from Kari and Wyn Korpikoski - 188 19th Avenue
- B2. Letter from Catherine & Craig Rosnuk - 30 Anglin Drive
- B3. Letter from JSW & Associates - NE Corner Yonge & 19th Avenue
- B4. Letter from B. Charters - 15 Long Hill Drive
- B5. Letter from Keith Lathem - 102 19th Avenue
- B6. Letter from Heidi Kreiner-Ley re: 15 Long Hill Drive
- B7. Follow-up letter from Keith Lathem - 102 19th Avenue

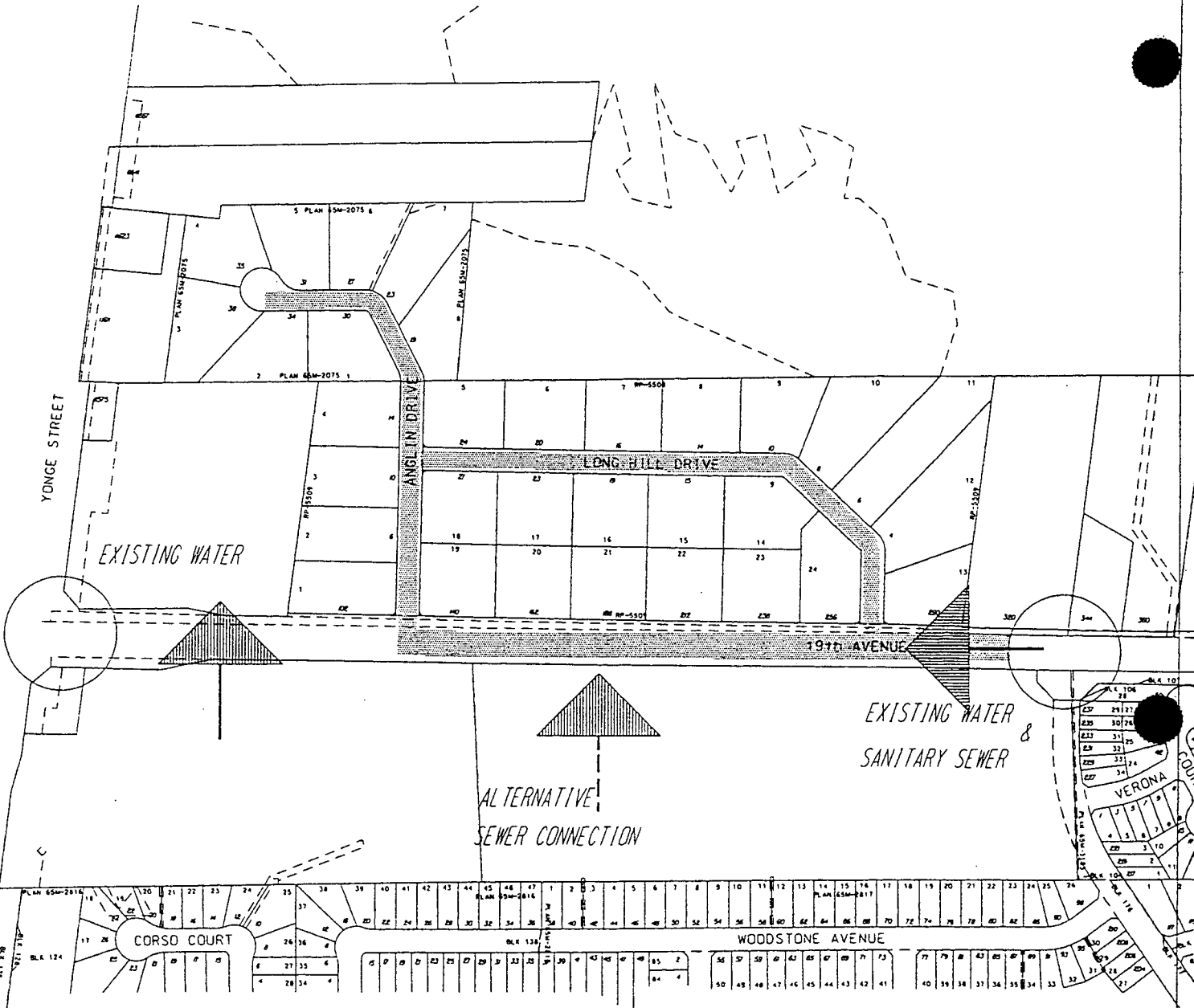
Drawings

Map 1 - Location Map on Front Cover

Map 2 - Proposed Subdivision 19T-98012

Map 3 - Potential Water Extension

Map 4 - Potential Sanitary Sewer Extensions



POTENTIAL SANITARY SEWER EXTENSIONS



MM/VP

SRP 99.003

MAP 4

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 BLOCK 10 DOO.UF.LA
 TOWN OF RICHMOND H
 PLANNING & DEVELOPMENT

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Appendix "A"

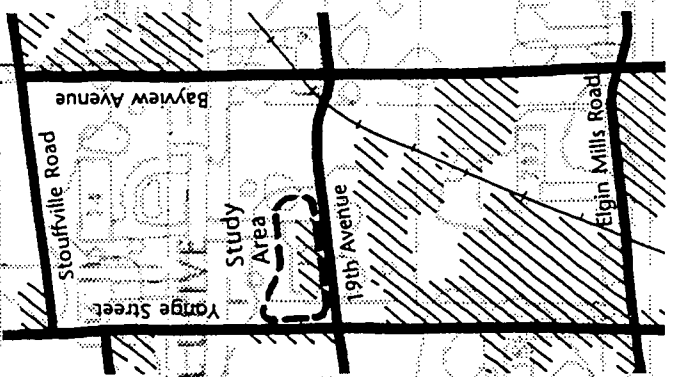
SRP. 99.003

File (s) Doo-UF-LA

Town of Richmond Hill

Anglin and Long Hill Drive Neighbourhood Infill Study

The Planning Partnership
December 1998



YONGE STREET

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1. Introduction

In December, 1997, the Town of Richmond Hill retained The Planning Partnership to prepare a demonstration plan, design guidelines and a draft zoning by-law to guide municipal staff in their review of applications for infill development in the Nineteenth Avenue, Anglin/Long Hill Drive neighbourhood in accordance with the land use policies of Official Plan Amendment No., 138: the Jefferson Secondary Plan. This neighbourhood will be defined by the valleys to the north east and by the 2 major arterials: Yonge Street to the west and 19th Avenue to the south. Additional guidelines for the development of the Yonge Street corridor lands would relate to those land uses that are not within the existing residential area.

Infill areas are created in a municipality to address issues related to the integration of older historic development that occurred at low densities, usually before water and sewer were available. Land uses - housing, shops and industries, schools and other institutions that were established on wells and septic tanks require physical changes to integrate them into the contemporary envisions of the suburban expansion which has begun to envelop them.

Development of this nature has an open character; in residential areas, houses have wide sideyards and frontages, roads are usually built to rural standards and trees and shrubs have grown between houses over the years. Frequently, there are distant views, a rare phenomenon in contemporary suburbia. Such areas are distinctive, highly attractive as living places and as they become surrounded by contemporary development at 6 to 10 times their density they become increasingly unique.

Of and by themselves, the continuation, at a limited scale, of such pockets is not a great detriment to a contemporary community. It offers a particular and unique housing product in a marketplace that has become homogenous. What does, in fact, occur in these areas is pressure from the residents to sever and subdivide. In these cases, the municipality has the responsibility to ensure that redevelopment is in the best interests of all, respects tenets of good subdivision design and attempts to maintain some of the more unique aspects of the existing community.

The questionnaire circulated to the neighbourhood indicates that many of the residents were in favour of development at the outset. In attempting to set out an appropriate scale of lots and houses for redevelopment, the study considered that it was important to maintain some aspects of the key attributes on streets with existing housing. These attributes include: wide frontages, deep setbacks and significant vegetation. It was considered that any backlot development could establish a new character without detriment to the existing neighbourhood.

It is apparent with current development economics, the maximum frontage for existing streets that was considered to be viable was 50 feet, a full 30% larger than the standards that would be applied to backland lotting. Development controls for these streets should also attempt to achieve a higher standard of landscaping on these streets than the normal requirement.

This report summarizes the study process and conclusions.

2. Meetings with the Residents

An initial meeting was held with the residents to introduce the study and to solicit initial comments and concerns in March 1998. Subsequent meetings were held with residents in July and November 1998 to present alternative plans for the infill and redevelopment possibilities in the area, as well the development process itself and two major development projects which were being considered in the area.

3. Survey of Residents

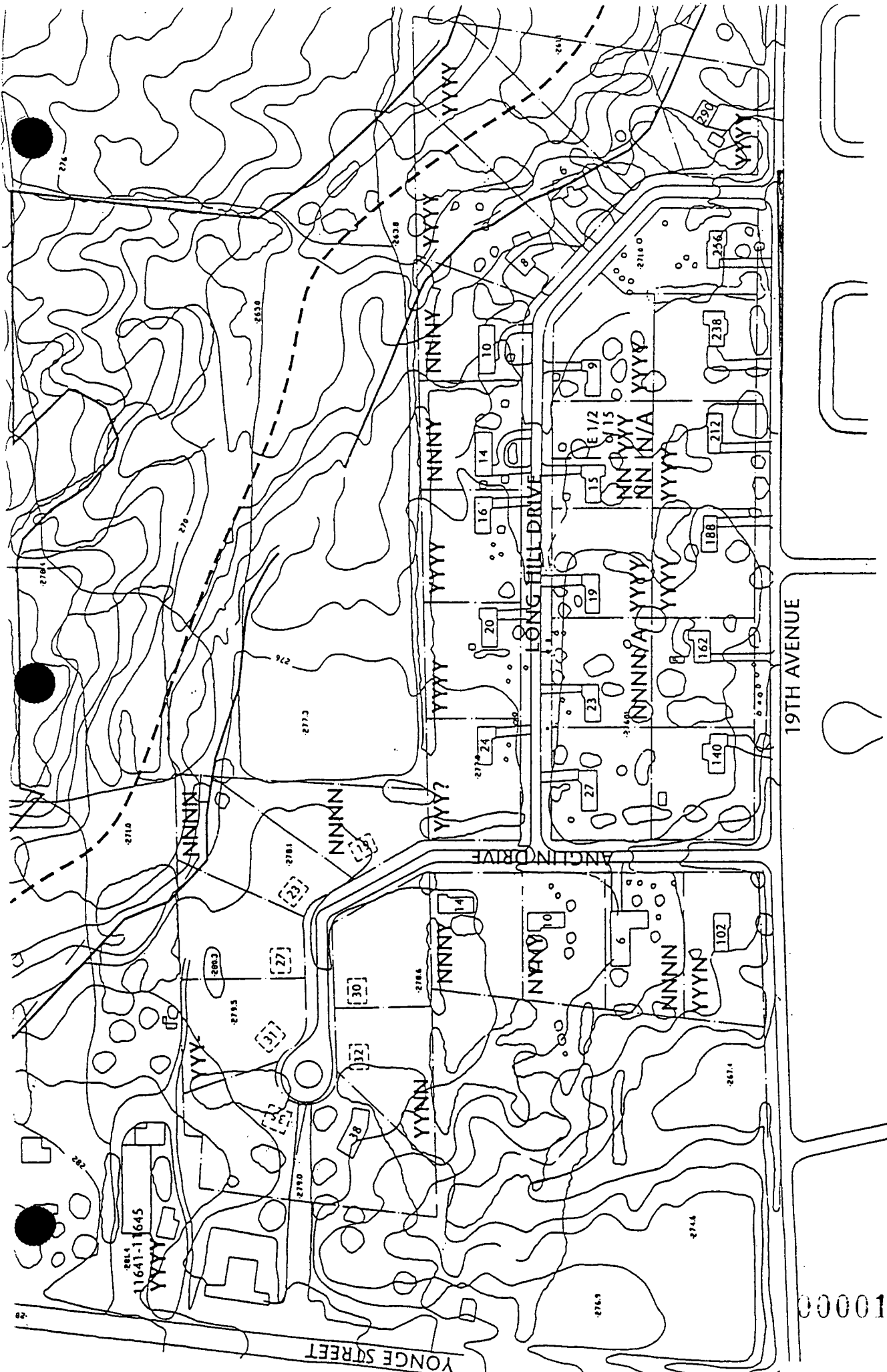
Following the March public meeting, a questionnaire survey was distributed to all residents in May 1998. The questionnaire requested a response to 5 questions:

1. Are you in favour of additional houses on your street?
2. Are you in favour of additional houses in your neighbourhood?
3. Would you like your property to be part of a plan for additional housing?
4. If a portion of your property were redeveloped for additional housing, would you stay in your present house?
5. How important are the following neighbourhood characteristics. Rank them as:

- | | |
|---|---------------------------------------|
| 1 | very important |
| 2 | somewhat important |
| 3 | not important |
| | ditches, instead of curbs and gutters |
| | mature trees and tree stands |
| | semi-rural /country like character |
| | setback to houses from road |
| | space between houses |
| | size of houses |

Figure 1 indicates the response to the first four questions. About half of the respondents indicated that they are not in favour of additional houses on the street or in the neighbourhood.

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Y=yes
N=no

- Questions:
1. Are you in favour of additional houses on your street?
 2. Are you in favour of additional houses in your neighbourhood?
 3. Would you like your property to be part of a plan for additional housing?
 4. If a portion of your property is redeveloped for additional housing, would you stay in your present house?

Anglin/Long Hill Drive Infill Survey Responses

The Planning Partnership

Figure 1

Residents were asked to rate as very important, somewhat important and not important, various characteristics of the neighbourhood. Most felt that the mature trees, setback to houses from the road, space between houses and size of the houses were either very important or somewhat important.

The following table summarizes the response of the ratepayers at the beginning of the study in the spring of 1998 to the value of various neighborhood characteristics:

	1 very important	2 somewhat important	3 not important
ditches instead of curbs and gutters	1	7	11
mature trees and tree stands	10	7	4
semi rural country like character	10	3	6
setback to houses from road	9	7	3
space between houses	10	8	1
size of houses	6	12	2

4. Neighbourhood Character

The area is characterized by large residential lots, typically one acre in size, with houses sited within one half of the lot frontage and built over the last two decades. Houses and properties are very well cared for. The streets are rural in character with ditches and no sidewalks. There is significant landscaping on private lots and street trees along both Anglin and Long Hill Drives.

There are two undeveloped parcels of land, one of which is now the subject of an application by developers before planning staff. These are located behind houses on the north side of Long Hill Drive and on the north east corner of Yonge and

Nineteenth Avenue. Opportunities for access into these parcels is indicated by black arrows on the Figure 2. The heavy black line indicates the limitations to access.

5. Design Principles

A number of principles have directed the preparation of infill scenarios for this neighbourhood.

Minimize direct access to Nineteenth Avenue

Nineteenth Avenue is a regional road for which there are plans for widening using land on the south side. Driveway access will be limited in any redevelopment scenarios to ensure efficient traffic operations.

Protect against noise from Nineteenth Avenue and Yonge Street

Scenarios for infill development should include a landscape or built form buffer to protect the neighbourhood from traffic noise along Nineteenth Avenue. Development on Yonge Street should be oriented so that private outdoor areas are protected from traffic noise.

Minimize disruption to areas with steep slopes and natural features

The most appropriate plans for redevelopment of the north east corner of Yonge Street and Nineteenth Avenue would create minimum disturbance to the steep site slopes. No development should be permitted over the established top of bank to the floodplain of the Rouge River tributary that defines the north and east edge of this neighbourhood.

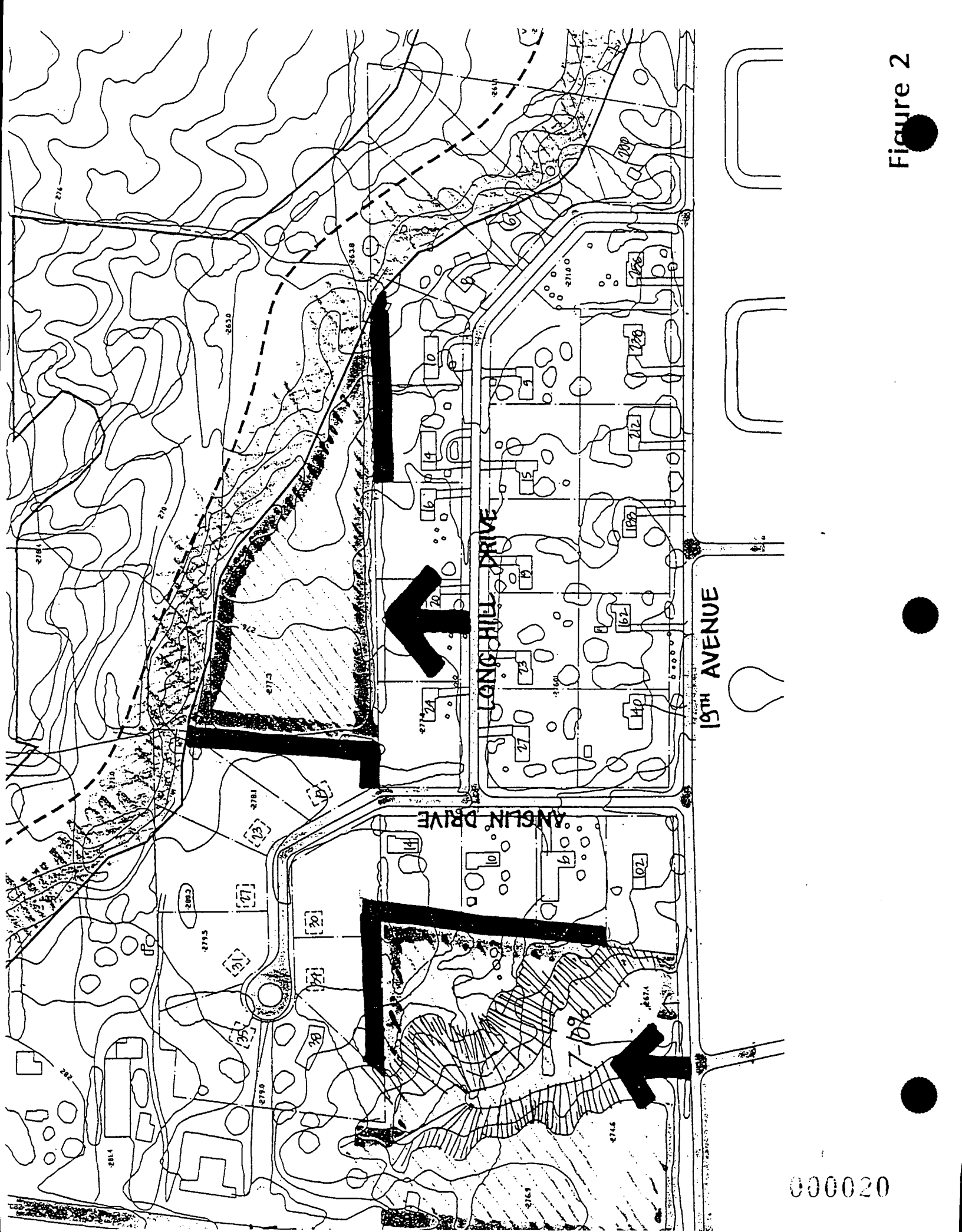


Figure 2

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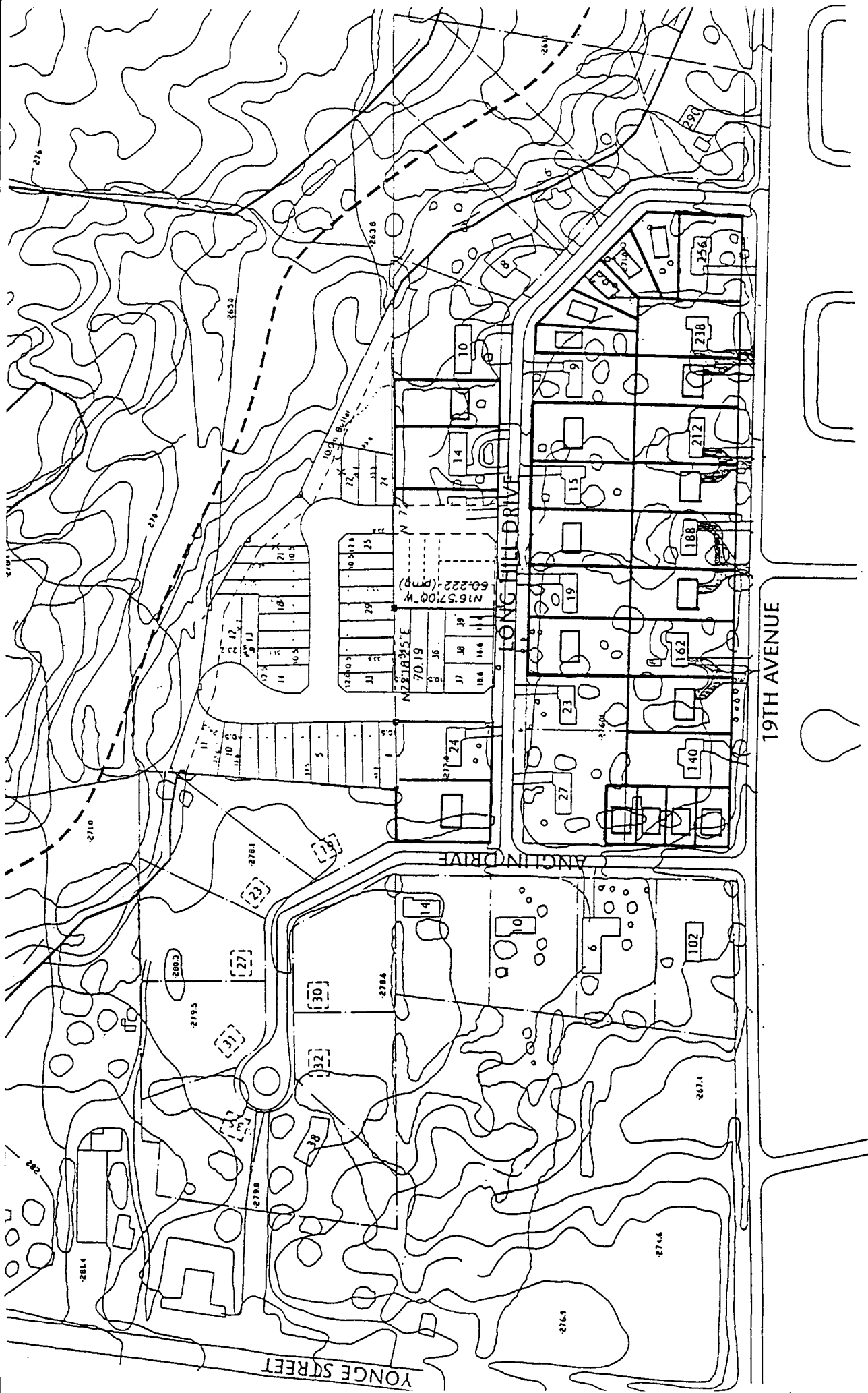
Establish appropriate development and redevelopment standards on existing streets.

New development should be designed to blend with existing conditions. This includes separation between units, extensive trees and other vegetation and deep setbacks from the streets to the houses.

6. Basic Infill Scenario

Most of the lots in the neighbourhood have significantly wide side yards with a potential for development of additional houses to infill those lots. Figure 3 demonstrates an initial and basic infill scenario showing additional houses located on 100 ft. sidelots of existing houses with 200 ft. frontages, whose residents were in favour of infill development. The figure also indicates the proposal of Crossmar Investments Ltd. to develop vacant lands to the north of Long Hill Drive with a loop street which accesses through Lot 20 and will exit in the future through Lot 16. In December 1998, Crossmar proposed for discussion in plan with 39 lots, including three 16.6m lots on Long Hill Drive and 36 Lots on lands behind with minimum frontages of 10.5m. Figure 3b indicates how further intensification could occur on Long Hill Drive if any 2 owners combined to create a "close" - a grouping of houses with 27 metre deep lots around a small parkette.

Nineteenth Avenue is a regional road. The Region will permit no further access driveways than presently exist. While initial reaction from the Region suggests that, while the concept of several common accesses from one driveway is acceptable, (as shown on Figure 3) relocation of these access points would have to be determined at the time of individual development applications.



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Figure 3

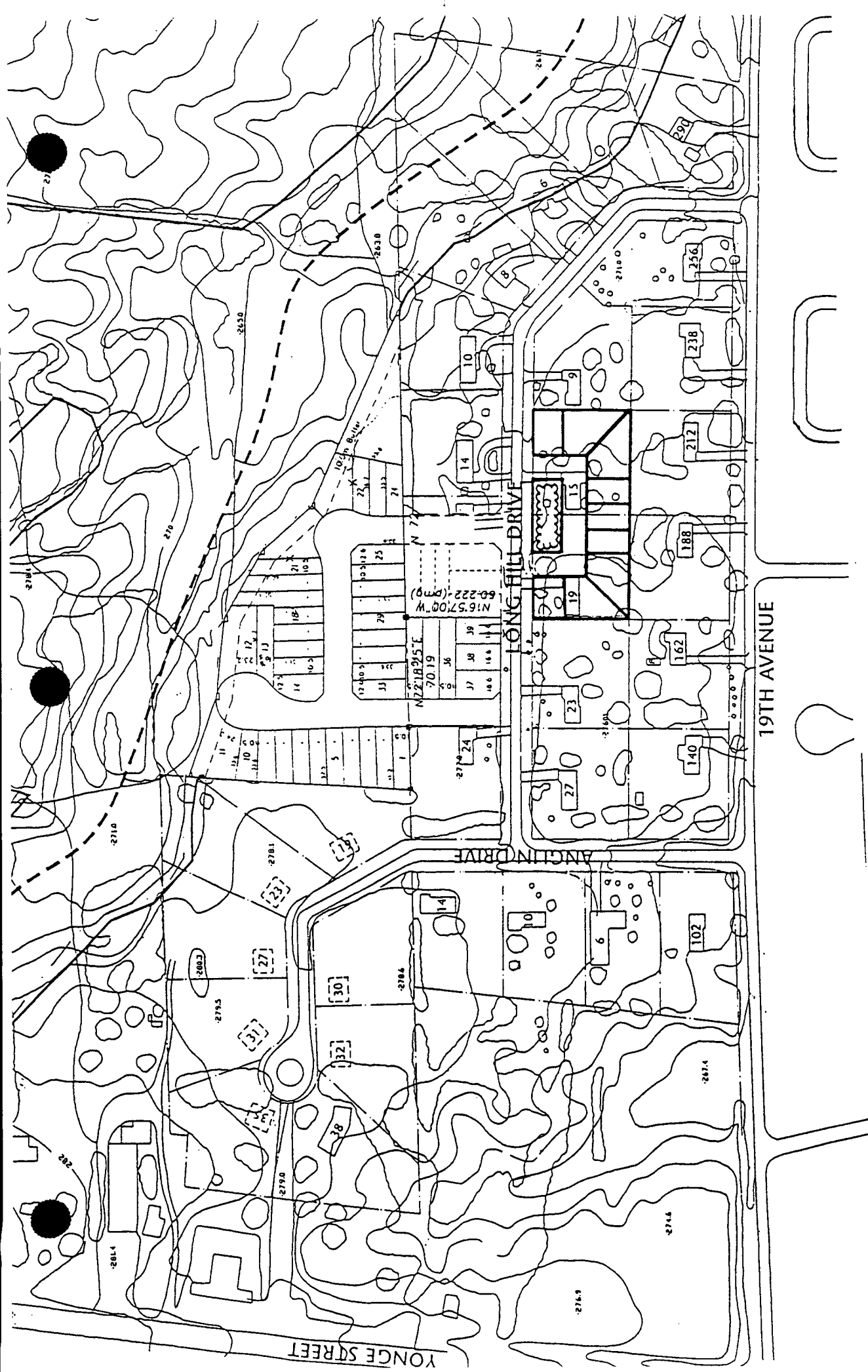


Figure 3b

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7. Redevelopment Scenarios

Area bounded by Long Hill Drive and Nineteenth Avenue

Two further scenarios demonstrating more extensive redevelopment of the Nineteenth Avenue frontage were prepared. These would require extensive co-operation between existing owners. Figure 4 indicates a new public cul-de-sac road providing access for the complete redevelopment of existing properties on Nineteenth Avenue. A slight modification of this scenario shown on Figure 5, illustrates the further extension of the new public road to Long Hill Drive enabling the redevelopment of yet another property whose owner was in favour of residential infill.

Area north of Long Hill Drive

Two scenarios (Figures 6 and 7) demonstrate redevelopment of this area, in a more extensive form than presently proposed by Crossmar on their own lands. The scenarios include the rear yards of Lots 10 through 16 on Long Hill Drive. It should be noted that at the time of the survey, Lots 10 and 14 were not in favour of their inclusion in an infill scheme.

Figure 8 illustrates the development of Lot 31 on Anglin Drive in which the owner has shown interest by the further extension of the cul-de-sac to the north through Lot 31 and into the adjoining property which fronts on Yonge Street. In this scenario Yonge St. frontage would become commercial/residential use accessed from Yonge Street. Figure 9 shows an alternative to the development of this Yonge Street property which is independent of Lot 31. Anglin Drive provides access either on a public or condominium road to backland. Such a proposal would eliminate the redevelopment potential of Lot 31 on its own, unless Lot 31 and Lot 27 could combine to provide infill potential.

No proposal should be considered which links Yonge Street through to Anglin Drive creating a "cut-through" and unduly increasing through traffic infiltration into the neighbourhood.

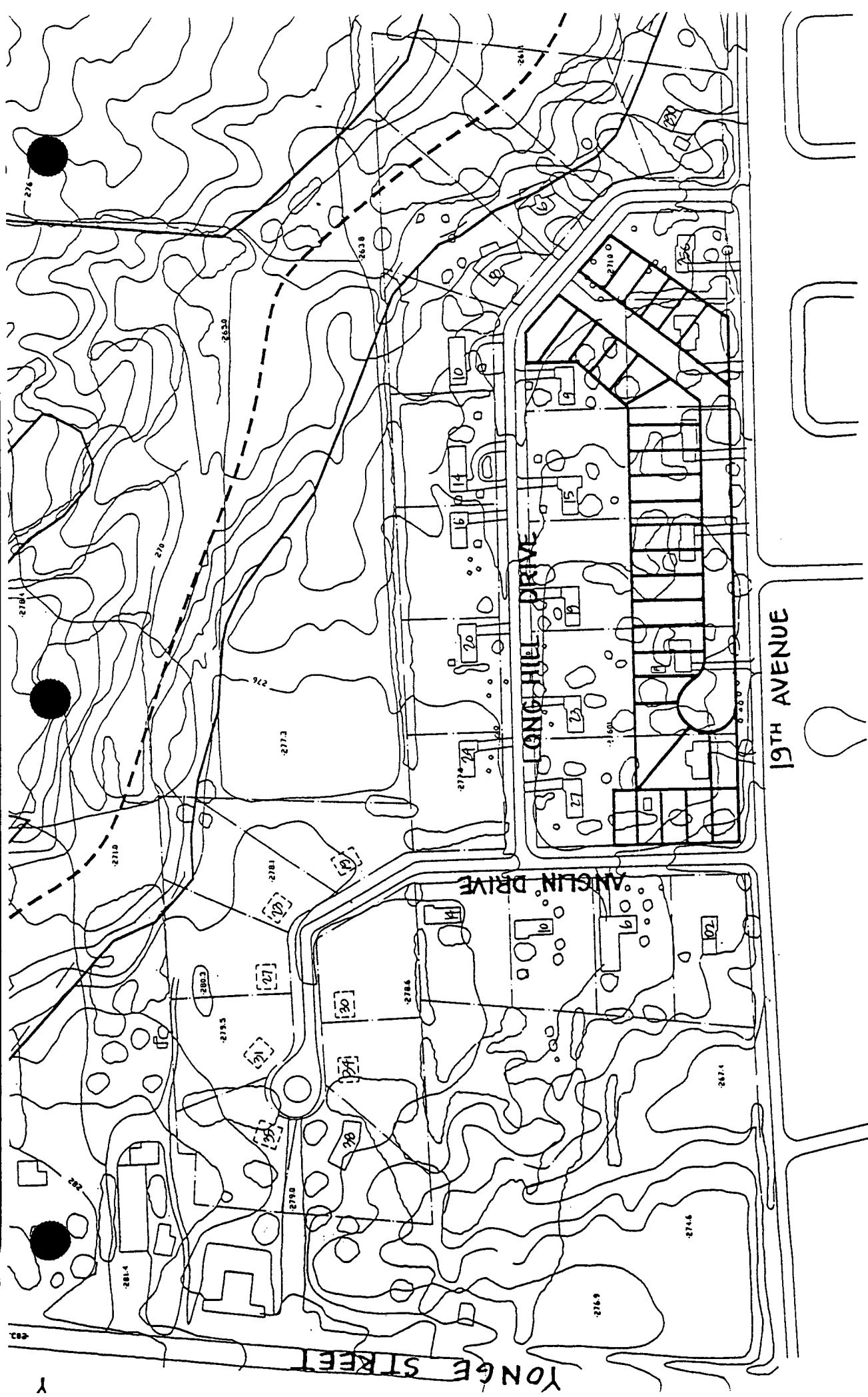
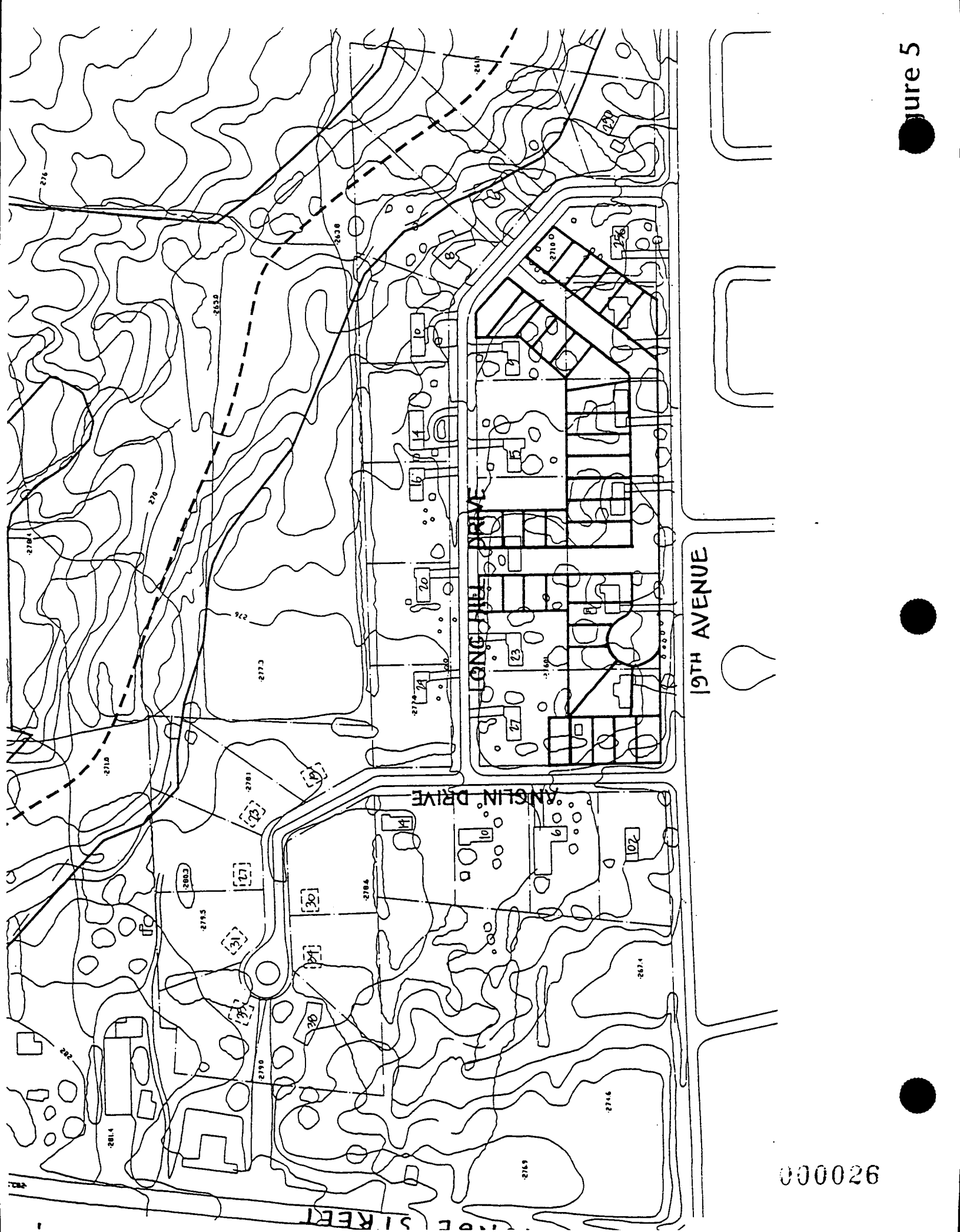


Figure 4

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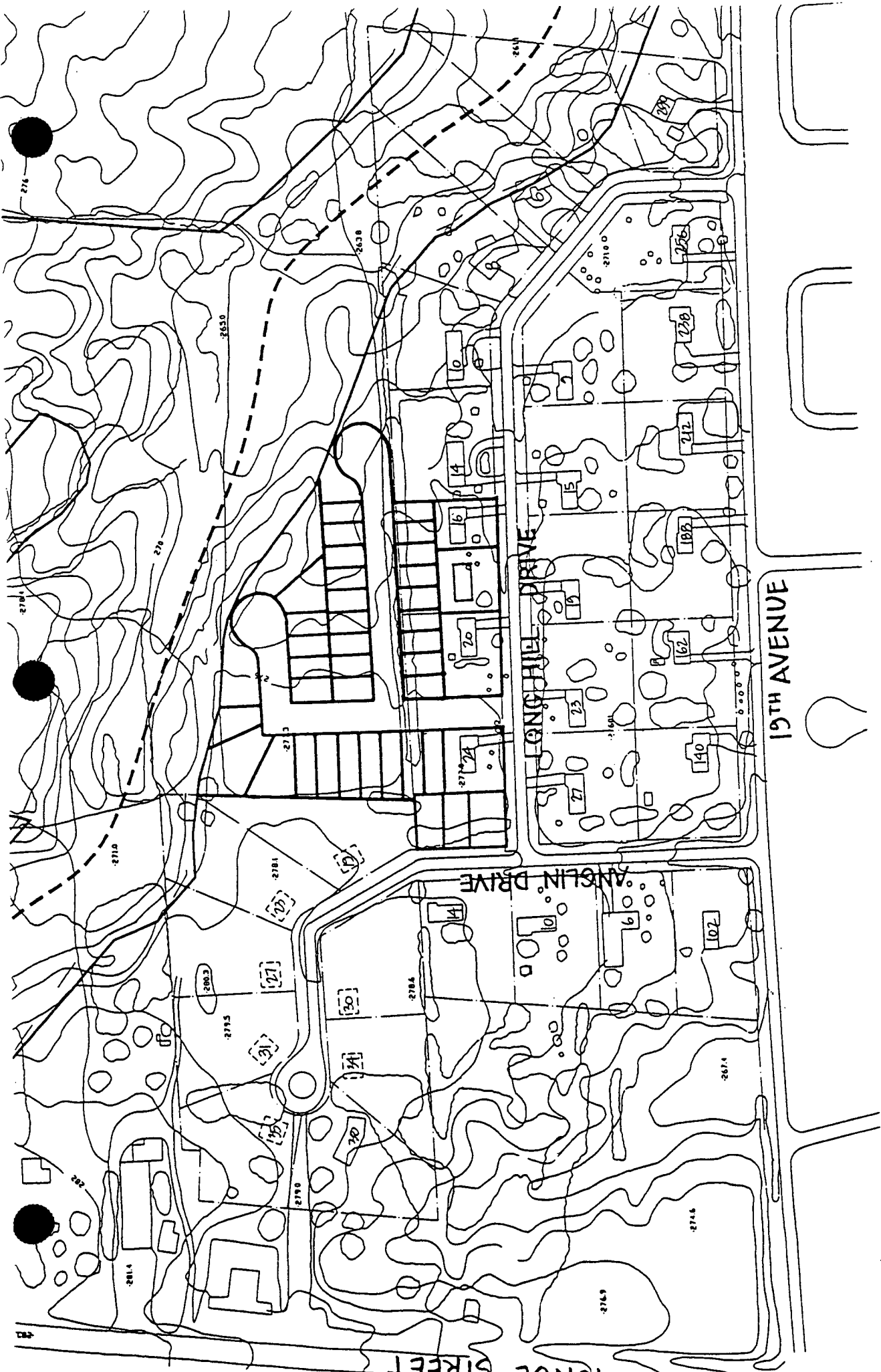
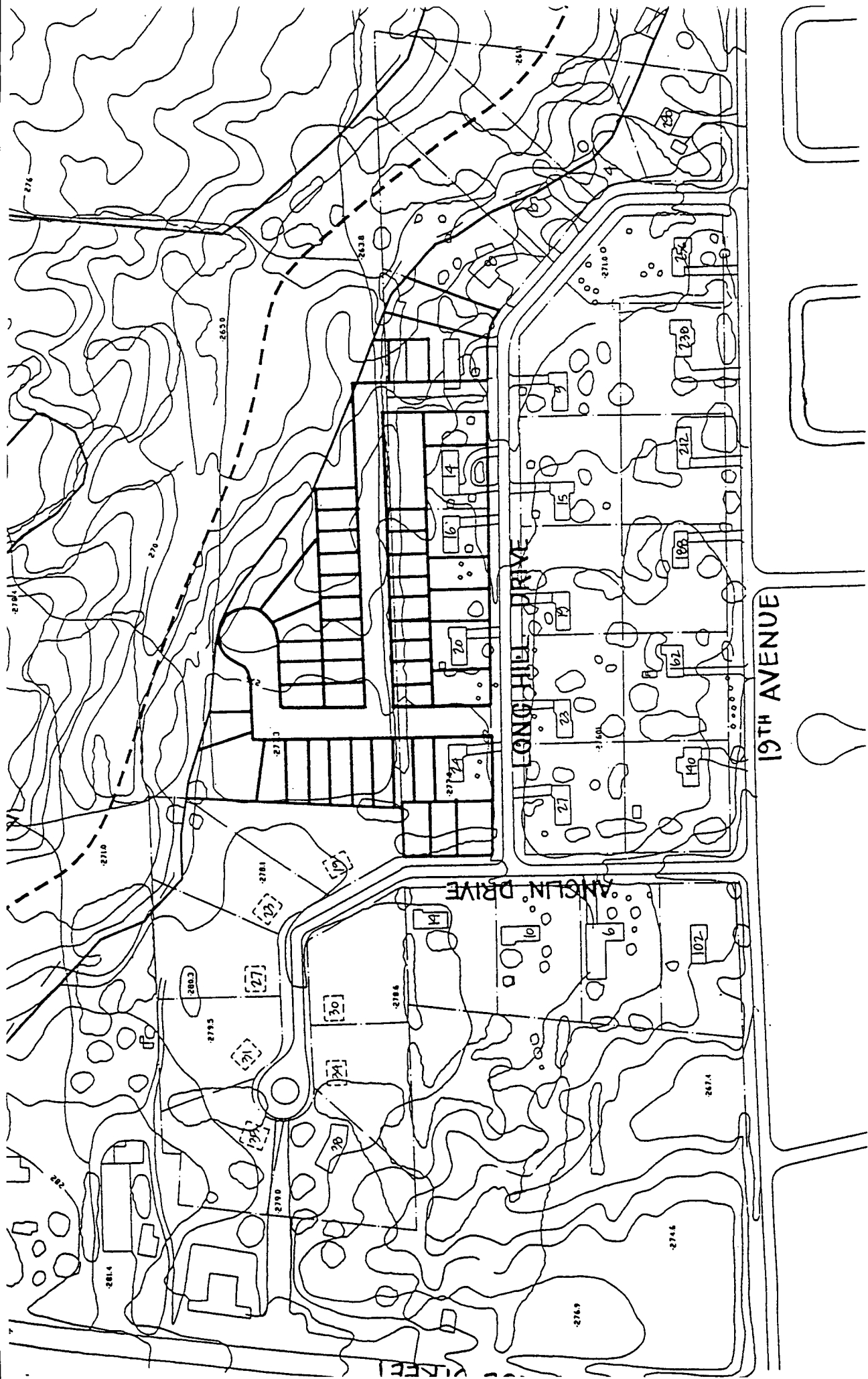


Figure 6

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Figure 7

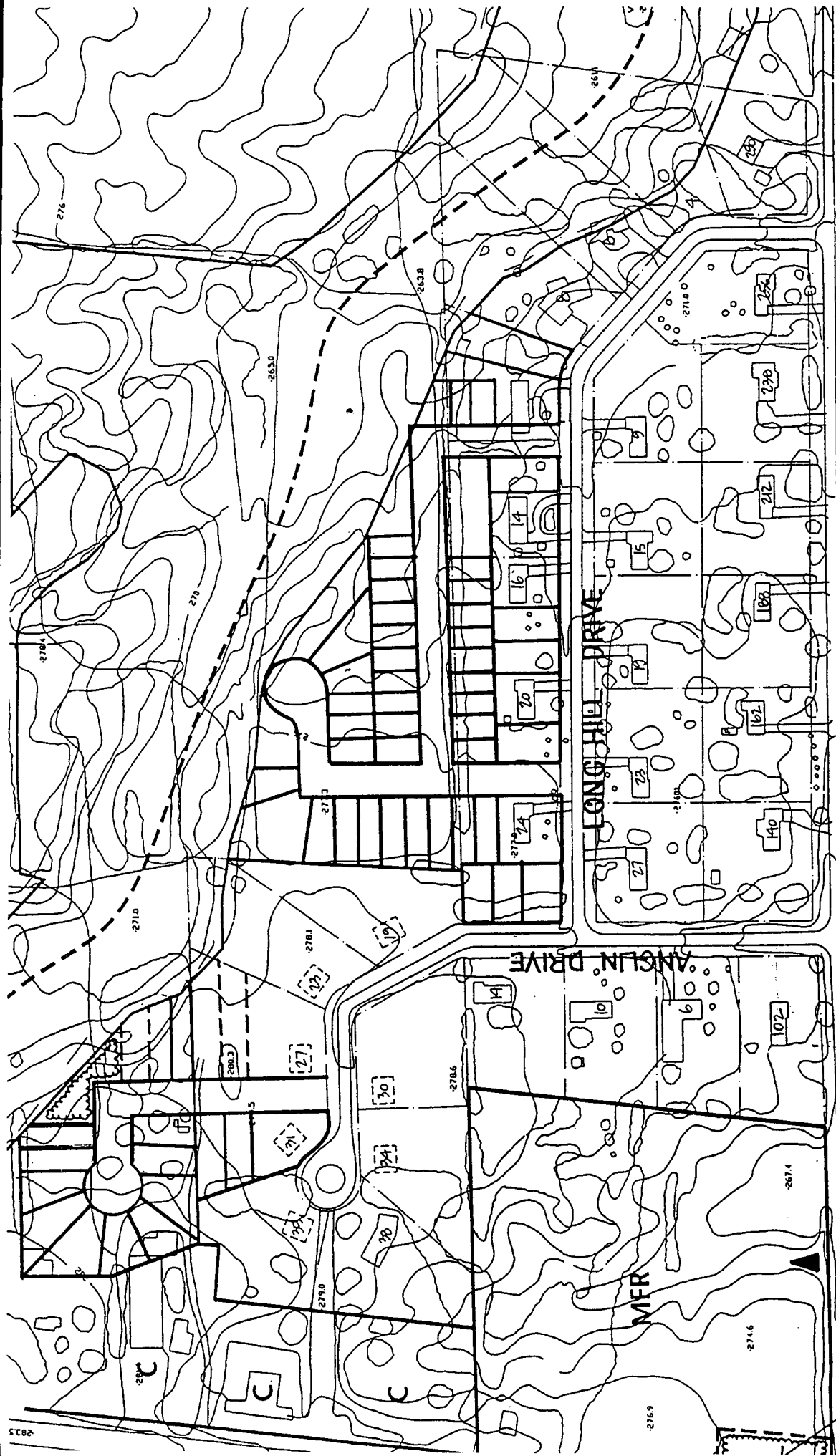
The 3.8 ha parcel fronted by Yonge Street on the west and Nineteenth Avenue on the south is bisected by a valley of 6 to 7 metres in depth. The site is designated for multiple residential development with a combination of medium (50 uph) and low (15uph) densities, the higher densities on the Yonge Street frontage. Any proposal for this development should be accompanied by a preliminary grading sketch indicating how the terrain would be addressed; this becomes particularly important when small detached units are proposed at medium densities. Layout should favour lot frontages onto the 2 arterials to encourage quieter backyards and a front to the main streets. Where flankages are provided onto these arterials the facades should be articulated with windows and doors.

Any development scheme proposed by the landowner for the block bounded by Yonge Street and 19th Avenue should take into consideration potential lotting at the rear of lots on the west side of Anglin Drive and indicate how these lands might be subdivided.

The location of the 5% open space (+/- 0.2ha) to be provided on the site should be evaluated in terms of the grading plan proposed; it should have both optimum visual and functional accessibility to the development proposed and ideally equally to the community and municipality at large. A possible location for this open space is illustrated on Figure 8.

Within the single family detached area, two scales of unit frontages are proposed. On existing streets, Anglin and Long Hill, the minimum proposed frontage on the existing 200 ft. frontages is proposed to be 15.2m (50') to maintain some reasonable frontage reduction which is not radically different from the existing large houses. On these streets an increased setback varying from 4.5m to 7.5m to the unit will help to maintain some further sense of front yard spaciousness to better integrate infill units with existing houses.

On newly created streets accessed from existing ROW's, infilling should be permitted on lots as small as 26 metres in depth with 13.5 metre frontages (wide-shallows). New street widths could be provided at 18 metre widths.



19TH AVENUE

LONG HILL DRIVE

ANSLIN DRIVE

MFR

potential open space location

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While the initial terms of reference suggested that a demonstration plan could be provided at the end of the study, it became apparent through the course of the study and through the examination of the range of options which existed, that no single optimum demonstration plan could be established. The future pattern of redevelopment in this area will be established by the residents themselves who propose development based on their ability to accommodate each other as potential development partners and the timing of such proposals. Adjoining owners on Long Hill would still be able to subdivide their existing lots into four 50' lots or combine to create the form of "close" indicated in Figure 3b.

8. Zoning By Law

At the present time, the subject lands are subject to By-law 2325-68 of the former Township of Markham. The subject lands are zoned RR1 by By-law 2325-68. The provisions of the RR1 Zone are below:

- lot area – 0.4 hectares (1.0 acre)
- lot frontage – 45 metres (150 feet)
- front yard – 7.6 metres (25 feet)
- side yard – 3.0 metres (10 feet)
- rear yard – 7.6 metres (25 feet)

This is an old by-law that is in the process of being phased out by the Town. The intent of the Town is to rescind By-law 2325-68 as it applies to the subject lands and to extend By-law 235-97 to the area.

By-law 235-97 is a by-law that covers lands between Bathurst Street to the west, Gamble Road/Nineteenth Avenue to the south, Bayview Avenue to the east and the southern boundary of the Summit Golf and Country Club to the north. These lands are located to the west of the subject lands, to the west of Yonge Street.

This study proposes that all new development in the area be in the form of single detached dwellings. 15.0 metre frontage lots should be the minimum along Long Hill Drive and Anglin Drive. This study also recommends that a front yard setback that is consistent with existing development on both Long Hill Drive and Anglin Drive should be maintained as new infill development occurs. Lots having a frontage of 10.5 metres or greater are proposed along any new street, depending on individual site circumstances.

The specific zone provisions to implement this study's recommendations are below.

The R6 Zone

On the basis of the above, it is recommended that a site-specific R6 Zone be applied to lands which front on both Anglin Drive and Long Hill Drive. The R6 Zone requires a minimum lot size of 500 square metres and a minimum frontage of 15.0 metres. These lands would be subject to all of the other standards of the R6 Zone, except that the front yard setback would be increased to 7.6 metres (25 feet). This setback would apply to all components of the building, including the attached garage and currently applies to these lands. The exterior side yard setback should also be increased to 7.6 metres, if the exterior side lot line corresponds to the lot line abutting Long Hill Drive or Anglin Drive.

The R3 Zone or RSW1 Zones

It is recommended that either the R3 or RWS1 Zones be applied to the lands that will be developed on any new street. The R3 Zone requires a minimum lot size of 350 square metres and a minimum frontage of 10.5 metres. The RSW1 Zone requires a minimum lot size of 310 square metres and a minimum lot frontage of 13.5 metres. The main difference between a lot in the R3 Zone and the RSW1 Zone is the depth of the lot, which is typically 33.5 metres in the R3 Zone and 26 metres in the RSW1 Zone.

Appendix	<u>"B1"</u>
SRP.	<u>99.003</u>
File (s)	<u>D00-UF-LA</u>

Planning and Development Department.

RE: Residential Infill Study
Longhill/Anglin/19th.Neighbourhood
File No: D00-UF-LA

I have reviewed the Draft plan and did not agree with the options found on Figure 4 and 5. In my opinion it will create unnecessary road patterns. As a resident facing on 19th Avenue I would like to see an option of 4-6 accesses of 1 driveway, per 1 acre lot, otherwise it will not make any economical sense.

Yours truly; Kari and Wyn Korpikoski
188 19 th.Avenue, Richmond-Hill
Ontario, L4E 3P6

Kari Korpikoski

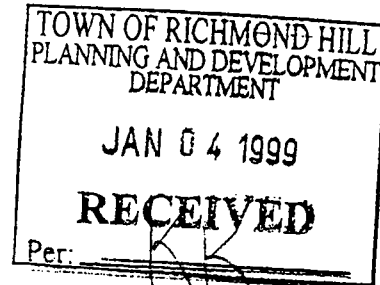
December 29, 1998

Appendix	<u>"B2"</u>
SRP.	<u>99.003</u>
File (s)	<u>D00-UF-LA</u>

Catherine & Craig Rosnuk
30 Anglin Drive
Richmond Hill, Ontario
L4E 3M5

December 18, 1998

The Town of Richmond Hill
P.O. Box 300
Planning and Development Department
225 East Beaver Creek Road
Richmond Hill, Ontario
L4C 4Y5



Fax: 905-771-2404

Attention: Janet E. Babcock, M.C.I.P., R.P.P., Commissioner of Planning and Development

Re: **Town of Richmond Hill**
Jefferson Development Area Secondary Plan
(Official Plan Amendment No. 138)
Residential Infill Study
Long Hill / Anglin Neighbourhood
Your File No.: D00-UF-LA

Dear Ms. Babcock:

It was a pleasure meeting with you on Wednesday, November 25, 1998. We are writing this letter to provide our comments on the infill study thus far.

OUR INVOLVEMENT

We took ownership of 30 Anglin Drive on July 2, 1998. We received notice and attended the meeting of November 25, 1998. We are concerned owners. Since this meeting, we have visited the planning department and have spoken to Mr. Kelvin Kwan on several occasions.

LETTER DATED MAY 1, 1998

- Letter provided by Mr. Kelvin Kwan
- "The comprehensive concept plans are to be based on the following principles: protection of natural environment, compatible in character with existing uses, limited access onto arterial roads, and efficient and safe street patterns"*
 - We agree with these principles, especially **Protection of Natural Environment**. There are many beautiful mature trees in the study area. We hope the Town realizes that these trees are significant and worthy of preservation because of its value as a wildlife habitat and its visual and aesthetic qualities.
- "Complete the enclosed survey..."*

000035

- We were unable to respond to the survey prior to May 15, 1998, however for your interest our input is as follows:

- 1. Additional houses on street NO
- 2. Additional houses in neighbourhood NO
- 3. Part of plan NO
- 4. Would we stay DEPENDS
- 5. Neighbourhood characteristics (where "1" is very important):
 - ditches instead of curbs & gutters 3
 - mature trees and tree stands** 1
 - semi-rural/country-like character** 1
 - setback to houses from road 2
 - space between houses** 1
 - size of houses 2

NEIGHBOURHOOD MEETING JULY 14, 1998

- Did not attend - not made aware of meeting
- Mr. Kelvin Kwan kindly provided us a copy of the handout.
- "Maintain the Anglin/Long Hill area as a unique neighbourhood in Richmond Hill"*
 - "Eclectic" or "Mish Mash" could be considered "unique" for some. The area is unique in character because of the variety of existing beautiful homes and treed landscape. It is not and should not be, a "tight" subdivision where each house is similar to another (there are already many "tight" areas like this in Richmond Hill). We would like to see the character of **classic homes in well-treed surroundings** maintained.
- "Figure 2"*
 - "Figure 2" shows undeveloped parcels of land. The lands bordering Yonge & 19th are zoned for MFR. We understand that Zetacorp is the developer that is presently submitting plans for the area. Is there a guideline for height of homes? Is there a guideline for maintaining a separation between zoning areas with mature trees and/or fencing?

NEIGHBOURHOOD MEETING NOVEMBER 25, 1998

- We received handout at the meeting
- "The larger the development / redevelopment proposal, the more efficient and aesthetic design solutions are possible"*
 - The concepts at this time show that as development / redevelopment proposals are larger, the smaller the lots will be (30' are proposed!), and thus the greater the chance for more trees to be destroyed and the effect on wildlife may be detrimental. How can this be more efficient and aesthetic to the existing neighbourhood where mature trees and tree stands, semi rural/country like character and spaces between the houses were priority neighbourhood

characteristics for the majority of owners (your survey)?

- "...it is important for new houses to take into account existing properties by maintaining good scale relationships..."*

 - How does one maintain good scale relationships when existing homes on 100' to 200' property lots are proposed to abut new homes on 30' lots (or even lots under 50')?

- "...trees are an important element.....loss of trees can greatly impact the character."*

 - Yes, we definitely agree. But in addition, loss of trees (especially mature trees) can greatly affect the existing environment. There is much wildlife (e.g. variety of birds, deer, rabbits) that presently co-exists with us in this neighbourhood. Removing trees will greatly affect the habitat and their home. This should be an important design consideration!
 - Please consider that a lot of the home are beautiful old homes, and as we present scenarios such as Figures 4 & 5, we will be removing these homes for lots that could be 1/6th the size of the average lot in the area and chances are no trees would survive the placement of new buildings.

- "How do I ensure that the character of my property is not negatively affected by infill development?"*

 - As the owner of 30 Anglin Drive we have a major concern on the effect of development around the cul-de-sac of Anglin Drive as we believe this area is especially unique to the other areas of Anglin and Long Hill. The majority of **cul-de-sac has been upgraded** with estate homes with landscaped lots. The curbing around the cul de sac homes is also upgraded. Figure 8 and 9, shown at the meeting but not issued, proposes a concept of a public road through the property of 31 Anglin Drive with 30' lots. This will negatively affect my property.

- "Are there measures that can be taken to reduce the impact of vehicular traffic and improve pedestrian use?"*

 - It is wonderful to see adults, children and babies in strollers taking daily walks around Anglin and Long Hill. Besides improving their health, they enjoy the tranquillity of the country setting and the presence of generally considerate local traffic. Adding new public roads will increase traffic. A design consideration should be that any new public roads should not encourage "short cut routes" or high traffic flow.

- "What aspects of my neighbourhood, important to me, have not been mentioned?"*

 - We don't regard this as a local area improvement - rather the opposite.
 - We are concerned with the effects on the habitat for the animals.
 - We have no problems with our well and septic system, and the majority of the family can drink well water but are allergic to chlorinated city water.
 - The effects of development on our well and septic systems.
 - The effect bringing water and sewers lines into the area to the visual appearance of our existing driveway (and the neighbourhood driveways) and who bears the

costs of bringing the driveways back to the present aesthetics.

- Will the neighbourhood become run down due to development?
- Maintaining the cul-de-sac for the safety of our child and future children.
- Ease and safety for future school bus routes.
- If development is to take place, then there should be a consideration for parkland.

"The installation of these (municipal) services is the responsibility of all benefitting land owners"

- What happens if we do not want these services?
- What happens if our neighbour requests these services - is our driveway / front area affected?

"Area Specific Development Charges"

- The Town has provided estimates for local improvements and front ending/cost sharing with Developer, what is involved in the Area Specific Development Charges option and are there any estimates?

"Servicing Costs"

- In addition to the servicing costs indicated, we understand that it may cost an extra \$3,000-\$4,000 for service connections to property line and an extra \$2,000-\$3,000 for services to the house. Are there any other possible surprise costs? (e.g. driveway & curb repair three times over?)

SUMMARY

Thank you for reviewing our comments. In summary, the role of the landowner, as indicated by the Town, is to provide input in the preparation of infill policies and concept plans. In addition to the above our input is as follows:

Ideas that would improve the neighbourhood:

- Trees or sound barriers dividing the commercial area and residential area of Anglin and Yonge; North section of properties 27 and 31 Anglin; and maintained or enhanced along 19th
- Mature trees separating R1 and MFR zones.

Types of development that will maintain the character of the neighbourhood:

- We prefer the maintenance of 1 acre lots.
- It has however been indicated to us that infill development may be inevitable, (especially as we understand that quite a portion of the homes/lots have already purchased by contractors and their family members). We understand that the Planning Partnership is proposing on recommending that if development is to occur then 50' lots on existing roads and 30' lots on new public roads are allowable. We suggest that to keep a character of the area and distances between houses, a **minimum of least 60'** for the whole area would be more than reasonable.

We do not believe that comprehensive back lotting or redevelopment proposals

will be able to maintain the character of the neighbourhood.

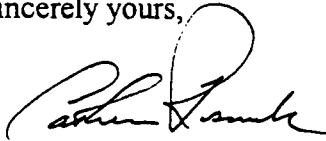
- Important qualities of the neighbourhood to be protected:*
 - The neighbourhood is presently described as “tranquil country setting”, “spruce trees”, “area of fine homes”, “quiet cul-de-sac of executive homes”, “prestigious area”, “fantastic homes”, “estate lots”. These qualities should be protected.
 - Important qualities: environment / wildlife
country / rural setting
mature trees
lot size
space between homes
 - Mature trees: trees take such a long time to grow and destroying them is such a waste. There should be guidelines established to **control unnecessary tree cutting**, ensure minimum frontage be adhered to, and lots should possibly vary to **maximize tree preservation**.

- What qualities should be changed:*
 - The existing “Gamble Park” should be made more prominent and be utilized and maintained as a park.

- What should be added:*
 - Traffic light at Yonge & 19th.

Thank you for your attention.

Sincerely yours,



Cathy & Craig Rosnuk

CR/cr

* c.c. Kelvin Kwan, Manager of Development (via Post)



Johnson Sustrunk Weinstein
+ Associates

Engineers
Landscape Architects
Planners

98-16

January 4, 1999

Town of Richmond Hill
P.O. Box 300
225 East Beaver Creek Road
Richmond Hill, Ontario
L4C 4Y5

Attn: Ms. Janet E. Babcock, M.Sc., MCIP, RPP
Commissioner of Planning and Development

Re: Residential Infill Study
Longhill/Anglin/19th Neighbourhood
Town of Richmond Hill
Jefferson Development Area Secondary Plan
(Official Plan Amendment No.138)
Town File No.: D00-UF-LA

Appendix "B3"
SRP. 99.003
File (s) D00-UF-LA

TOWN OF RICHMOND HILL
PLANNING AND DEVELOPMENT
DEPARTMENT

JAN 06 1999
RECEIVED
Per: _____

Dear Ms. Babcock:

Further to your letter of December 23, 1998 and your request for comments with respect to a finalized draft report prepared by the Planning Partnership for the above noted Study, we offer the following (using the draft report as a basis for discussion):

PARAGRAPH 1 - INTRODUCTION

1. This section of the report fails to mention that the driving force behind this study is OPA No.138, the Jefferson Secondary Plan. This plan was officially approved by By-law 151-96 of June 17, 1996 and certified by the Region of York on December 18, 1997. As stated in your cover letter, the Jefferson Secondary Plan OPA No.138 requires two things to be carried out prior to any re-development of infill lotting in the Longhill Drive, Anglin and 19th Neighbourhood. These are (1) a comprehensive concept plan for infill and (2) full municipal services. This information should obviously be stated in the introduction to the report to set the proper stage for the posturing, that has brought us to the point of requiring this study.
2. This study is not really generated by "principle pressure precipitated by two developer proposals", but by the fact that OPA No.138 now promotes the infilling redevelopment indicated within its' concept and residential policy. This is further supported by the Official Plan for Richmond Hill, Paragraph 2.2.1, as it gives priority to subdivision applications which represent infill development, as outlined in the Official Plan document. The fact is that the parcel at Yonge and 19th has little impact on the infill potential of the existing subdivision, particularly since all but one of the adjacent owners is not even interested in redevelopment.

3. The Study Area is really not defined and should be, utilizing OPA No.138 as a basis for reference.

The introduction goes on to say that the study considers certain attributes in the existing residential community as significant. Why the study made this determination and not the residents is unclear at this stage, since the next paragraph indicates the study team met with the residents and handed out a questionnaire, to determine what things were most important in the neighbourhood from their point of view. It should be pointed out there may be other issues that are more important to the residents than those simply listed in the questionnaire.

PARAGRAPH 5 – DESIGN PRINCIPLES

4. These “design principles” are items which appear to have been dropped into this design process, and are not always reflective of OPA No.138 or the Official Plan document, which sets the stage for the infill redevelopment. It is therefore unclear as to where these principles came from, and in some cases what affect they may specifically have on infill redevelopment. Of special note here is the statement regarding minimized disruption to areas with steep slopes and natural features. The paragraph following specifically talks about the north-east corner of Yonge Street and 19th Avenue proposed development, which in fact has no impact on the infill redevelopment, nor is it part of the infill Study Area. This statement should be removed from the report.

The Town is well aware, as should its’ consultants be, that the OPA reflects specifically two types of residential development permitted in the north-east corner of Yonge and 19th quadrant. These include low density and medium density residential areas. It is noted here that the area shown in the draft report containing 7 to 10% slopes was reviewed carefully with the TRCA and the Town during the OPA No.138 preparation, and was established as a residential development area after careful scrutiny by all agencies.

PARAGRAPH 7 – REDEVELOPMENT SCENARIOS

5. The 3.8 hectare parcel fronted by Yonge on the west and 19th on the south is really not part of the infill area, and it is unclear as to why the study would suggest design principles related to its’ development. Since the Town has been working directly with the developer and its consultants in the development of this property, they are well aware that none of the property owners backing onto the 3.8 parcel are interested in redevelopment, excepting the owner of Lot 102 on Anglin Drive. A potential lotting scheme for this development has been shown to the Town. In addition, the demonstration of the allocated open space at the corner of Yonge and 19th by the draft report really has little to do with the infill redevelopment on Longhill and Anglin, and therefore should be deleted from the study report.
6. The study in its consideration of redevelopment scenarios, notes that the extended local street at the end of Anglin Drive through to Yonge Street is creating a “short cut through” as shown in the OPA No.138 Plan. This demonstrates a lack of understanding of the OPA No.138 concept, which is basically to reinforce the village concept and provide direct access for many of its’ residents to the village centre vis-à-vis both a pedestrian system and local streets. Therefore this comment is again not in keeping with OPA No.138, and should be dropped from the study discussion or reconsidered.

We make these comments on behalf of Intracorp Corporation, acting as their consultants on the 3.8 hectare parcel at the corner of Yonge Street and 19th Avenue.

We trust these comments are of assistance and specifically provide input relative to the Infill Study and its relationship to the 3.8 hectare parcel.

Yours very truly,



Gary L. Heine, MCIP, RPP
Landscape Architect/Planner Principal

GLH/slv

c.c. Peter Campbell (Intracorp)
Glen Murphy (Intracorp)

Appendix	"B4"
SRP.	99.003
File (s)	D00-UF-LA
Faxed and Mailed	

December 29, 1998

From: B. Charters
15 Long Hill Drive
Richmond Hill Ontario
L4E 3M5

Town of Richmond Hill
P.O. Box 300
Richmond Hill Ontario
L4C 4Y5

Attention: Ms Janet Babcock
Commissioner of Planning and Development

Dear Ms Babcock,

Re: 15 Long Hill Drive - New Owner
Long Hill/Anglin/19th Neighbourhood
Residential Infill Study
Town of Richmond Hill

I am in receipt of the above-noted study. While I am a relatively new owner in the area (15 Long Hill Drive), I do have an interest in the future of the neighbourhood and it's redevelopment.

Firstly I wish to inform you that my responses to the survey questions, as outlined in Figure 1 of your study are all "yes" whereas the previous owner was of a negative opinion. Please take this into account in your report to Council and in future studies and reports.

I am in favour of redevelopment of the area and wish to work with the Town, the area residents and the developer to assure, not only for the good planning of the neighbourhood but to also assure financial viability is maintained for those owners who have invested in the neighbourhood.

Kindly keep me informed of any and all meetings or reports with respect to the area development.

Yours Truly,



B. Charters
15 Long Hill Drive
Richmond Hill Ontario
L4E 3M5

c.c. via fax
Kelvin Kwan, Planner, Town of Richmond Hill
Mike Mallett, Planner, Town of Richmond Hill

Appendix	<u>"B5"</u>
SRP.	<u>99.003</u>
File (s)	<u>D00-UF-LA</u>

102 Nineteenth Avenue
Richmond Hill, Ontario L4E 3M5

January 4, 1999

10219thd/e001

Janet E. Babcock, M.Sc., M.C.I.P., R.P.P.
Commissioner of Planning and Development
Town of Richmond Hill
P.O. Box 300
Richmond Hill, Ontario
L4C 4Y5

**Re: Residential Infill Study
Longhill/Anglin/19th Neighbourhood
Town of Richmond Hill
Jefferson Development Area Secondary Plan
(Official Plan Amendment No. 138)
Your File No.: D00-UF-LA**

Dear Ms. Babcock:

This letter is written in response to your letter and draft report on the above-noted matter which was hand delivered to my residence on December 23, 1998.

Following review of the contents of the report, which provided a fair account of the materials presented at the meeting with the residents which I attended, the following comments would address the remaining concerns:

1. The report does not address the protection of back lot development opportunities for the existing development where it is impacted by the two proposed subdivisions.

This is particularly a concern for the residents to the west and south of Anglin Drive as well as those residents to the north of Longhill Drive and to the east of Anglin Drive.

2. During the November 25, 1998 meeting with residents the Town Staff presented some costs associated with the development, servicing and division of existing lots. Those costs included a monetary charge of 5% of the value of each lot created, for park dedication. This cost represents between \$30,000 and 50,000 per existing lot Owner depending upon how the lot is divided.

Residential Infill Study
Longhill/Anglin/19th Neighbourhood
Town of Richmond Hill
Jefferson Development Area Secondary Plan
(Official Plan Amendment No. 138)
Your File No.: D00-UF-LA

Page 2

At the time the original subdivision was designed, approved and constructed a park dedication was set aside for the use of residents and the costs of that park was reflected in the original purchase price of the houses. That park, which was emphasized to me as the original owner of my house, still exists to this day although it has not been developed for any active use.

It is my contention that, since this park was given as a land area and paid for by my original purchase, it still represents an equitable dedication under the Planning Act for the me and the neighbourhood and that no further lands or monies should be required from the Owners of the original lots for which the dedication was made. It would seem that a second dedication or charge at this time would be 'double charging' which is not the intent of the Act.

I would like to thank the Town Council, Staff and the Planning Consultant for their efforts in providing information and assisting residents in dealing with very complex planning issues. I hope that you will retain some flexibility in evaluating future plans for infill development and for the equitable integration of development both in our neighbourhood and to the south of Nineteenth Avenue.

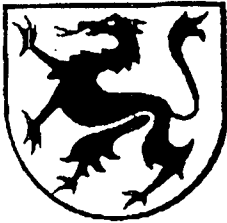
I trust that the attached comments will be of use and would like to receive any further revisions to this report that might be prepared prior to presentation as well as a copy of any staff reports that might be produced.

Your very truly,



Keith W. Lathem, M.Sc., P.Eng.
102 Nineteenth Avenue

Appendix	"B6"
SRP.	99.003
File(s)	DOO-UF-LA



JOSEPH KREINER REAL ESTATE LIMITED

10737 BATHURST STREET RICHMOND HILL, ONT. L4C 7V3 TEL: (905) 883-5551 FAX: (905) 883-3706

January 5, 1999

Faxed & Mailed

Town of Richmond Hill
 Planning Department
 P.O. Box 300
 Richmond Hill Ontario
 L4C 4Y5

Attention: Janet E. Babcock, M.Sc.M.C.I.P.
 Commissioner of Planning

Dear Ms Babcock,

Re: Residential Infill Study
 Longhill/Anglin/19th Neighbourhood
 Town of Richmond Hill
 Jefferson Development Area Secondary Plan
 (Official Plan Amendment No. 138)
 Your File No. DOO-UF-LA

Thank you for the above report which we received on December 23, 1998 and which you required a response by January 4, 1999. Upon speaking to Kelvin Kwan, he advised that our comments could be sent by January 7, 1999 and still be included in the report to Council.

As you are aware, I have been representing Mr. Luciano Stancich, who holds an interest in 15 Long Hill Drive. (east 1/2 lot 15)

Mr. Stancich is in favour of the study you provided and is interested in development in the area. He is wishing to work with his neighbours, the Town and the developer to assure the best planning and financial scenarios for the area are considered when development occurs.

.../2



MEMBER OF TORONTO REAL ESTATE BOARD

OVER FORTY YEARS OF DEDICATED REAL ESTATE SERVICE

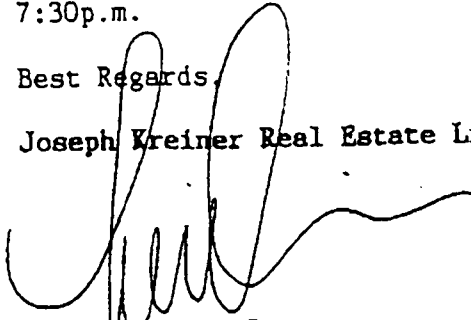
000046

Town of Richmond Hill
Attn: Janet Babcock
Longhill/Anglin/19th Neighbourhood
January 5, 1999
cont'd

Thank you for giving us the opportunity to be part of the infill and development process and we will be attending the January 20th meeting at 7:30p.m.

Best Regards,

Joseph Kreiner Real Estate Limited

A handwritten signature in black ink, appearing to read 'Heidi Kreiner-Ley', written over the typed name below.

Heidi Kreiner-Ley
Property Manager

/dr.

c.c. Luciano Stancich
Vito Spatafora

Appendix	<u>"B7"</u>
SRP.	<u>99.003</u>
File (s)	<u>D00-UF-LA</u>

Richmond Hill, Ontario

January 6, 1999

Janet E. Babcock, M.Sc., M.C.I.P., R.P.P.
 Commissioner of Planning and Development
 Town of Richmond Hill
 P.O. Box 300
 Richmond Hill, ON L4C 4Y5

**Re: Residential Infill Study, Longhill/Anglin/19th Neighbourhood
 Town of Richmond Hill, Jefferson Development Area Secondary Plan
 (Official Plan Amendment No. 138), Your File No.: D00-UF-LA
 and
 Proposed Plan of Subdivision of Part of Lot 56, Concession 1 E.Y.S.**

Dear Ms. Babcock:

We, as residents and property owners on Anglin Drive, have reviewed and discussed your letter and draft report of December 23, 1998. We have also viewed a Proposed Draft Plan of Subdivision of Part of Lot 56, Concession 1 E.Y.S. submitted by 730269 Ontario Inc., Zetacorp Inc., Kostas Kostopoulos and Demetra Kostopoulos.

Based upon our review of these documents we believe that the Proposed Plan of Subdivision fails to protect our ability to redevelop our properties to their highest and best use by preventing access to the back of our lots and by ignoring lotting patterns on adjacent lands. We would ask that the draft report and final planning documents contain appropriate conditions to ensure that any approved plans in this area provide the necessary opportunity for the development of our lands in future, including adequate access.

We thank you for the opportunity to review this material and ask for favourable treatment of this request.

Respectfully Submitted,

[Signature]
 14 Anglin Drive

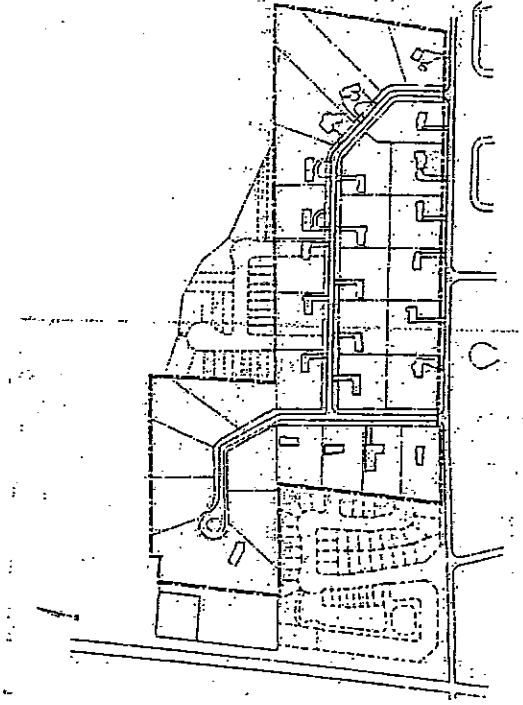
[Signature]
 10 Anglin Drive

[Signature]
 38 Anglin Drive

[Signature]
 102 19th Avenue

ANGLIN DR. / LONGHILL DR. / 19TH AVENUE

Town of Richmond Hill



ARCHITECTURAL DESIGN GUIDELINES for INFILL DEVELOPMENT

Prepared by:
John G. Williams Limited, Architect
40 Vogell Road, Unit 46
Richmond Hill, ON L4B 3N6
(905) 780-9536

June 2004
Our Ref. No. : W-1239

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TOWN OF RICHMOND HILL
 PLANNING AND DEVELOPMENT
 DEPARTMENT
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1. INTRODUCTION

These Architectural Design Guidelines have been prepared on behalf of the Town of Richmond Hill to establish architectural design and site planning criteria to guide residential infilling and re-development occurring within the Anglin Dr. / Longhill Dr. / 19th Ave. neighbourhood. The subject lands are located on the north side 19th Avenue just east of Yonge Street within the Town of Richmond Hill as shown on the Key Map below.

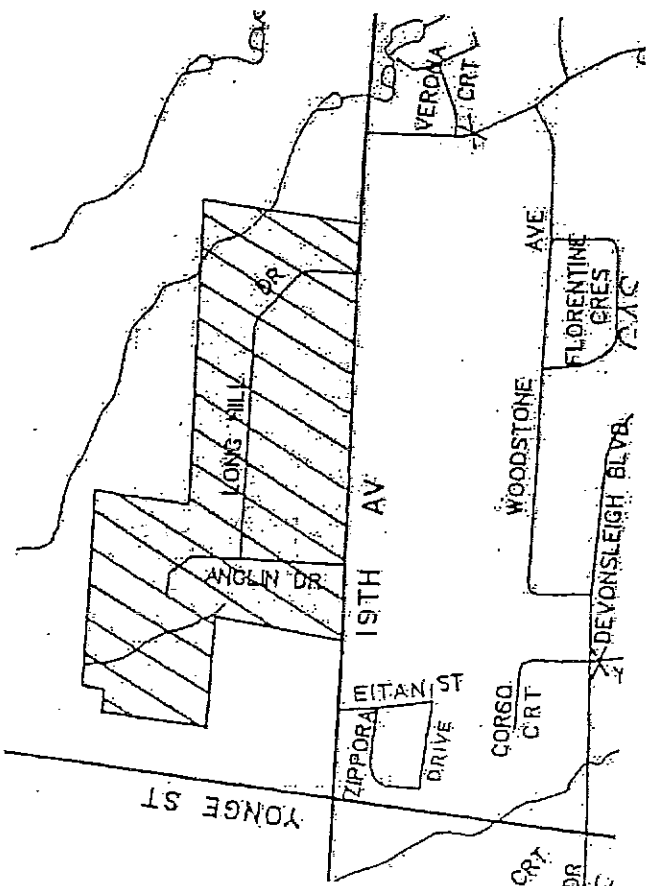


Figure 1.1a - Key Map

The area is comprised of existing residential lots that are approximately one acre in size with homes typically sited on one half of the lot. Existing housing in this area has developed over approximately the past fifty years as an eclectic mix of bungalows and 2 storey dwellings with no predominant architectural style. Due to recent changes in zoning and the addition of full municipal servicing to the area there is currently redevelopment pressure on the Neighbourhood which has triggered the process of infill development.

Infill development will occur incrementally within the neighbourhood over time as landowners make the decision to sever or subdivide their properties. Due to the expected wide variety of builders and designers in the Neighbourhood it is expected that a mix of dwelling styles and designs will occur. This is in contrast to typical contemporary suburban developments which generally have a single developer/builder and hence a uniform streetscape appearance. In an effort to ensure that the neighbourhood develops a positive visual identity, it is important to encourage new housing which is compatible in terms of its streetscape appearance and quality throughout the neighbourhood.

1.1 Objectives of the Guidelines

The objectives of these Architectural Design Guidelines are:

- To establish a framework of architectural design criteria to assist builders/designers in preparation of dwelling designs and site-planning.
- To promote dwelling designs which have a high standard of exterior architectural quality.
- To encourage visual compatibility among new dwelling designs.
- To encourage the siting of new dwellings which are sensitive to and compatible with adjacent uses.
- To establish design requirements for dwellings in prominent locations, such as corner lots.
- To diminish the visual impact of garages within the streetscape.
- To establish procedures for submission, review and approval of building designs.

These guidelines and their interpretation by the Control Architect are not intended to discourage design creativity or innovation. Proposed designs which are not in total compliance with the guidelines will be considered based on their design merits, provided the spirit and intent of the guidelines is preserved.

2. ARCHITECTURAL CONTROL PROCESS

2.1 Preliminary Review

- An initial meeting with the Builder / Designer and the Control Architect should take place early in the preliminary design process.
- Submissions for preliminary review should include:
 - Site Plan
 - Floor Plans
 - All Exterior Elevations & Details
 - Materials & Colours
- Refer to Appendix A "Architectural Design Review Checklist" and Appendix B "Typical External Material and Colours Schedule".
- Plans will be reviewed and marked-up if necessary then returned to the Builder. Builders should allow 2-3 days for comments.
- Where there is a dispute between the control architect and the Builder concerning the application of these guidelines, a meeting with the Town of Richmond Hill Planning and Development Department may be necessary.

2.2 Final Review and Approval

- The Builder is required to submit to the Control Architect for final review and approval, the following:
 - 6 sets of site plans; (Site plans shall be certified by a qualified engineer and should be at a minimum scale of 1:250.)
 - 4 sets of working drawings; (Working drawings must depict exactly what the builder intends to construct. All exterior details and materials must be clearly shown on the drawings. Working drawings shall show true grade conditions.)
 - 2 sets of the exterior colour schedule; (A typed colour schedule indicating the colour, type and manufacturer of all exterior materials).
- The applicant should allow up to 5 working days for final approvals.
- Minor redline revisions to the plans may be necessary.

2.3 Town of Richmond Hill Approval

- It is the Builder's complete responsibility to ensure that all plans submitted for approval fully comply all applicable regulations and requirements including zoning and building code provisions. All fees relating to the Control Architect's review and approval are due in full at time of final architectural control approval. Refer to Appendix C "Fee Schedule"
- Submissions should be made to:

John G. Williams Limited, Architect
 40 Yogell Road, Unit 46
 Richmond Hill, ON L4B 3N6
 Tel: (905) 780-9534 Fax: (905) 780-9536

2.4 Monitoring For Compliance

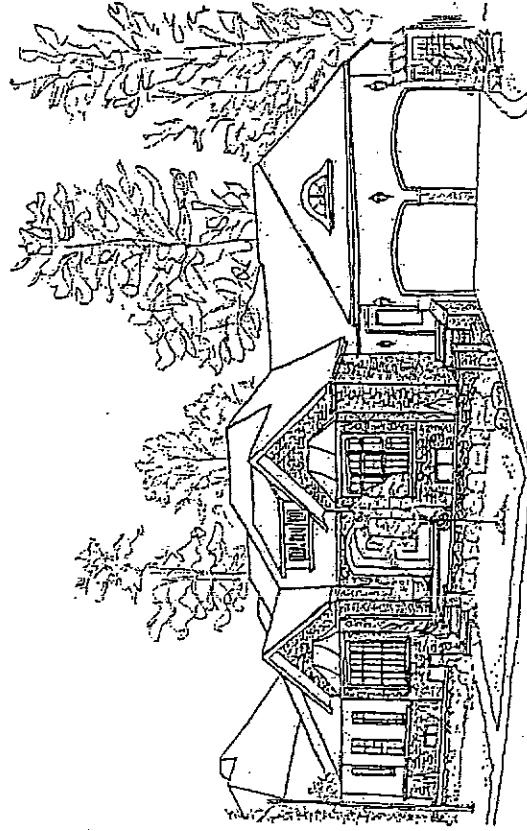
- The Control Architect will conduct periodic site inspections to monitor development. Deviations in construction from the approved plans which are not in compliance with the Architectural Review Guidelines will be reported in writing to the Builder and Town.
- The Builder will respond in writing within 7 days of notification of their intention to rectify the problem.
- The Town may take appropriate action to secure compliance.

3. NEIGHBOURHOOD STREETSCAPES

The following section outlines some of the common architectural design objectives for the entire neighbourhood. Builders should keep these objectives in mind when designing new housing.

3.1 Design Quality

- Publicly exposed elevations shall incorporate adequate massing, proportions and wall openings to avoid large blank façades.
- The use of high quality, low-maintenance building materials is required to contribute to a positive visual character for the neighbourhood.
- All dwellings within the community shall provide street-facing façades designed to minimize the prominence of the garage.
- New dwellings should enhance and intergrate with the surrounding neighbourhood.

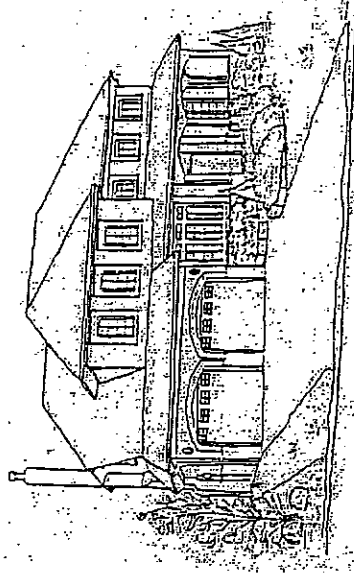


High Quality Dwelling Designs Are Required

3.2 Community Safety

To promote a safe, pedestrian-friendly neighbourhood, dwelling designs will be encouraged to incorporate principles of CPTED (Crime Prevention Through Environmental Design), such as:

- De-emphasizing the presence of the garage within the streetscape.
- Providing ample windows facing public areas to encourage eyes on the street.
- Providing large, usable front porches, porticos or courtyards to promote interactive outdoor spaces.
- Ensuring all entries to the dwelling are well lit.
- Avoiding main entries to the dwelling which are deeply recessed or hidden from the street.

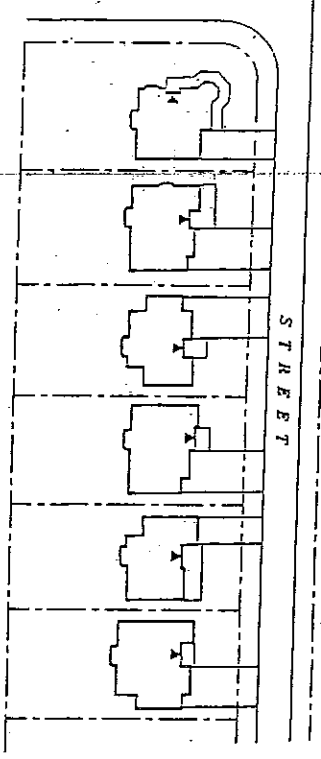


Dwellings Should Be Designed To Encourage "Eyes On The Street" (i.e. Ample Fenestration; Usable Porch, Portico or Courtyard)

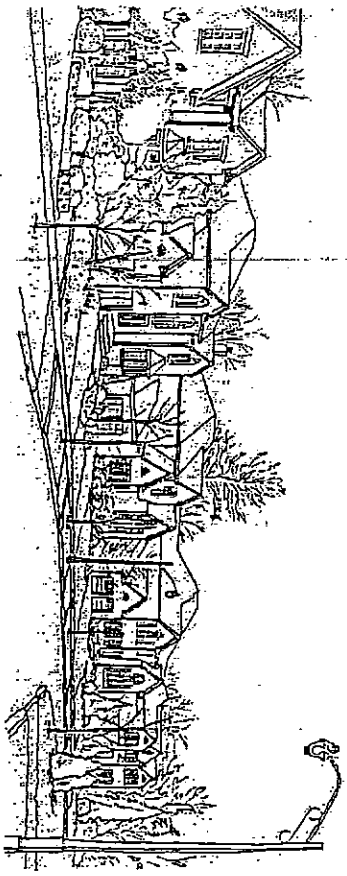
3.3 Street & Building Relationships

- The front façade of the dwelling should face the street and should be designed to visually dominate the garage.
- Dwellings should be sited with due regard for the front yard setbacks of adjacent dwellings in order to provide consistency in the appearance of the streetscape.

- Although the zoning by-law permits a 4.5m minimum front yard setback, within the Neighbourhood a minimum front yard setback of 7.5m is required. However, greater setbacks may be requested since generous front yards are desirable in preserving the existing character of the Neighbourhood
- Projections into the front yard, such as porches and bay windows are encouraged. These design elements have a beneficial impact on the streetscape by creating pedestrian-friendly and visually inviting streets.
- For corner lots, both street frontages shall be addressed in an appropriate manner. Refer to Section 6.1 Corner Lot Dwellings.

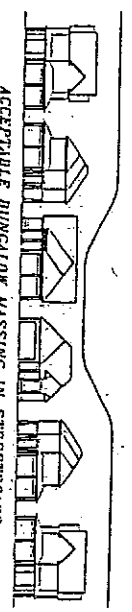
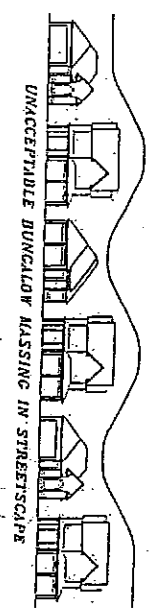


Desirable Siting Relationships Between Adjacent Dwellings



- 3.4 Facade Variety Within the Streetscape
- The visual appeal of the streetscape is enhanced when the arrangement of the dwellings is ordered with respect to model variety, massing, height and repetition within the group.
- Variety of architectural expression among publicly exposed elevations should occur within each street block to provide visual interest / relief.
 - Identical or similar dwelling elevations shall not be permitted directly adjacent or directly opposite one another. To further promote visual diversity along each street, a minimum of 2 dwellings must occur between similar elevations.
 - Dwellings adjacent to one another should be compatible in massing, size and height. Extreme variation in massing is discouraged.

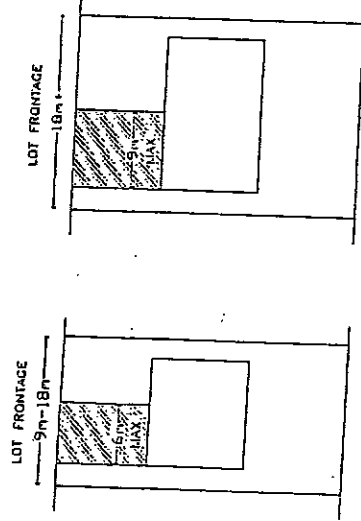
Bungalows should be designed with steep roof forms, raised front elevations and/or lofts to provide a visual transition between adjacent 1 and 2 storey dwellings and to minimize their apparent variation in height and massing.



Adjacent Dwellings Should Demonstrate Appropriate Transitioning of Massing and Height

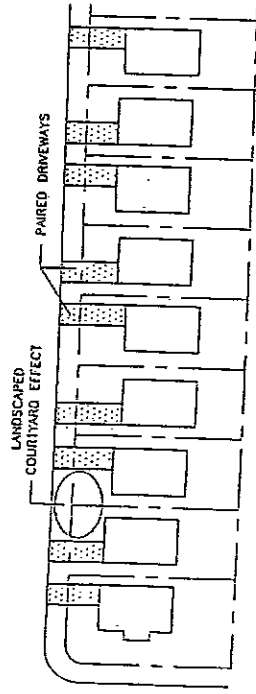
3.5 Driveways

- Driveways should be no wider than the garages they serve.
- The maximum width of a driveway for lots with frontages 9.0m to 18.0m is 6.0m.
- The maximum width of a driveway for lots with frontages in excess of 18.0m is 9.0m.



Driveway Width Standards

- Paired driveways, separated by a landscaped strip, are encouraged as they promote the joining of front yards thereby promoting opportunities for larger continuous landscaped areas.
- Paired driveways will not be required where constraints are present such as sloping site conditions or conflicts with street furniture.

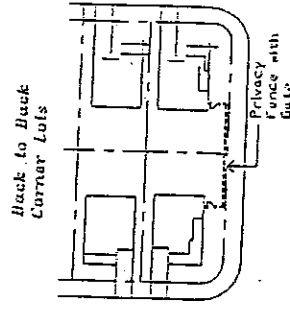
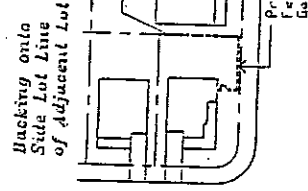
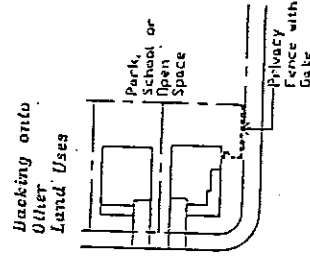


Paired Driveway Locations are Desirable

- Driveways for dwellings adjacent intersections, parks, open space or other non-residential land uses should be located as far from the adjacent use as possible.
- Driveways located at the top of T-Intersections should be located to the outside of the pair of dwellings which terminate the view.
- Driveway slopes between garage and street are to be as shallow as possible and in accordance with municipal standards.
- The width of driveways accessing rear yard garages shall be kept to a single lane width until the driveway nears the garage door.

3.6 Corner Lot Fencing

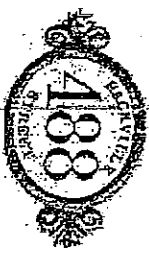
- Corner lot privacy fencing is encouraged to be provided by the builder to enclose private rear yards otherwise exposed to flanking streets.
- The location of corner fencing should follow the flankage lot line from the rear corner of the lot to a point approximately 1.5m beyond the rear corner of the dwelling, then returning to the side wall of the dwelling. Refer to diagrams below for recommended corner lot privacy fence / gate location.
- All fencing shall be in compliance with applicable municipal standards.



Corner Lot Privacy Fencing Locations

3.7 Municipal Address Signage

- Municipal address numbers shall be provided in a design complementary to the character of the dwelling.
- The municipal address shall be located prominently on the front facade of the dwelling or garage.
- Acceptable designs include:
 - Etched masonry plaques set into the wall cladding;
 - Pre-finished ceramic plaques set in a wrought-iron bezel;
 - Pre-finished metal plaques.



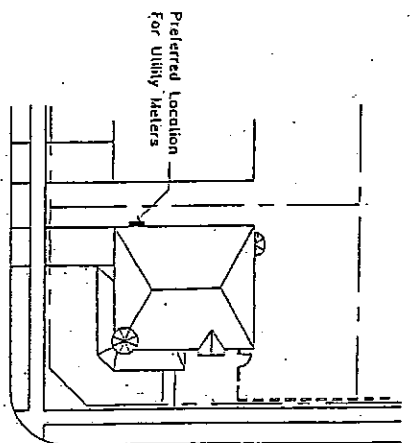
Examples of Municipal Address Plaques

3.8 Streetscape Elements

- Streetscape elements occur within the R.O.W. and include but are not limited to street trees, light standards, hydrants, street signs, community mailboxes, transformers and other street furniture.
- The builder is required to coordinate dwelling site plans with all streetscape elements located within the street R.O.W. to ensure there are no conflicts between dwelling, driveway, walkway or other dwelling site plan component and streetscape elements. This requirement is the builder's sole responsibility.

3.9 Utility and Service Elements

- To reduce their visual impact, utility meters or service connections for hydro, water, natural gas and telephone should be located out of direct view from the street, such as the side yard wall face.
- For corner lot dwellings, utility meters should be located on the interior side yard wall face; where utility meters must be located on flanking walls exposed to public view, they should be screened architecturally to reduce their visibility from the street.
- Air conditioning units, dryer vents, furnace vents and fireplace vents should be located away from the dwelling's front or flanking yard. If this is not possible, they shall be screened with landscaping or fencing.

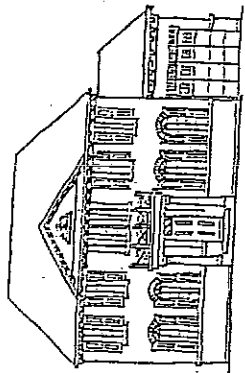


Meter Locations for Corner Lots

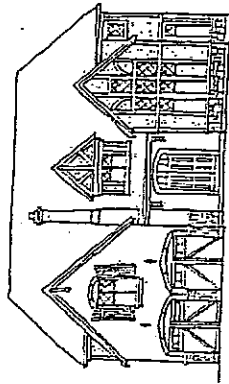
7. ARCHITECTURAL DESIGN CRITERIA

4.1 Architectural Styles

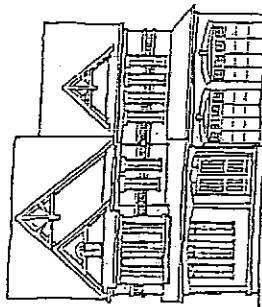
- A range of compatible architectural styles will be permitted within the Neighbourhood. Styles derived from the classical periods of architecture will be encouraged (such as Georgian, Tudor, Victorian, Arts & Crafts, French Eclectic, etc).
- Ultra-modern house designs will be discouraged.
- Mixing discordant architectural styles together within a single building is not permitted.



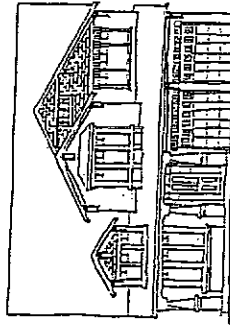
Georgian



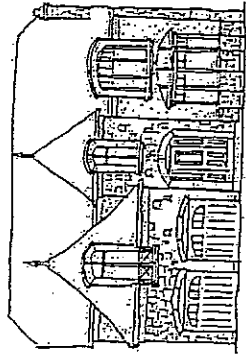
Tudor



Victorian



Arts & Crafts

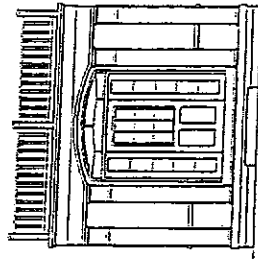
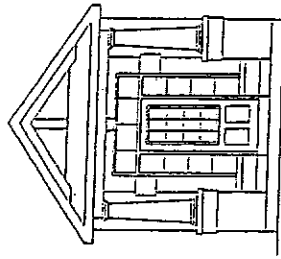


French Eclectic

Conceptual Images of Architectural Styles

4.2 Main Entrances

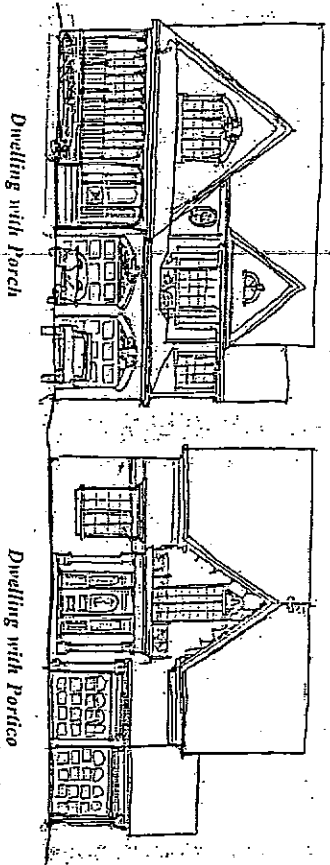
- The main entrance to the dwelling should be directly visible from the street and convey its importance as the focal point of the façade.
- Weather protection at entries should be provided through the use of covered porches, porticos, overhangs or recesses.
- The front entry design and detail should be consistent with the architectural style of the dwelling.
- Natural light at the entry should be provided through the use of glass sidelights and transoms.
- Houses should be designed so that front entries do not require more than a few steps up. Large concentrations of steps at the front entry are to be avoided.



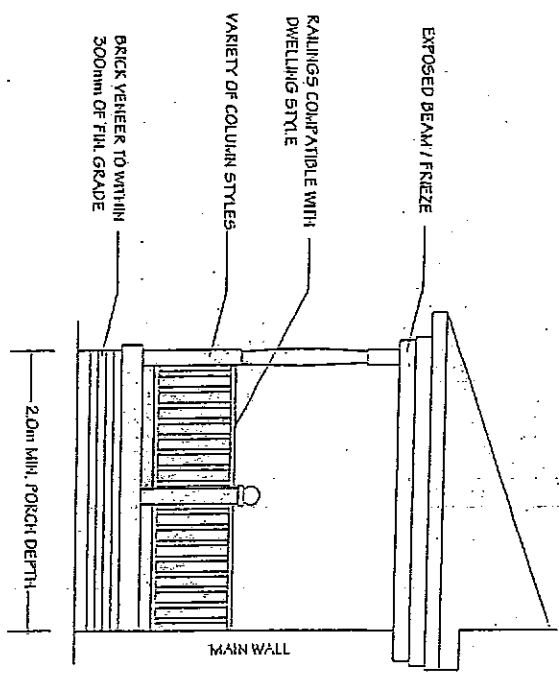
Main Entrances should be the Focal Point of the Dwelling

4.3 Porches

- Front porches, porticos, courtyards or patios are encouraged for all dwellings as they help to promote safe, socially interactive and pedestrian-friendly residential streets by providing an outdoor amenity area, shelter from inclement weather, and a linkage between the public and private realm.
- The design of a porch or portico shall be consistent with the architectural style of the dwelling.
- Porch depths should be at least 2.0m to facilitate comfortable seating.
- The size of the porch/portico and its components (columns, piers, brackets or moldings) shall be proportional to the scale of the dwelling.



- Porch/portico roofs shall generally be supported on a continuous frieze resting on columns.
- Where porches are elevated, masonry veneering shall be applied to the front and sides of the porch face to ensure no more than 300 mm of exposed concrete foundation wall is visible.
- Steps accessing the porch should be poured-in-place concrete with the exposed sides finished to match the front façade cladding.
- Where railings are required, they shall be of traditional design appropriate to the style of the dwelling.



Example of Porch Details

4.4 Exterior Materials

- i) Wall Cladding
 - A high standard of quality, design, detail and variety of wall cladding is encouraged to attain a harmonious blend of textures and colours within the streetscape.
 - The following main wall cladding materials are permitted:
 - Brick should have heritage-based tones with a smooth, textured or weathered appearance. Calcite or concrete brick will not be permitted.
 - Stone should display heritage styles, colours and textures including limestone, natural stone, cultured stone, and manufactured stone. (Certain colours, styles and textures of manufactured stone may be inappropriate such as bright white, pink, blue or green tones or stone with a rough granular texture).
 - Stucco in natural tones with appropriate trim detailing such as detailed mouldings or half-timbering.
 - The use of vinyl siding as a main cladding material is not permitted and should only be used in areas of limited public exposure where supporting masonry is impractical.

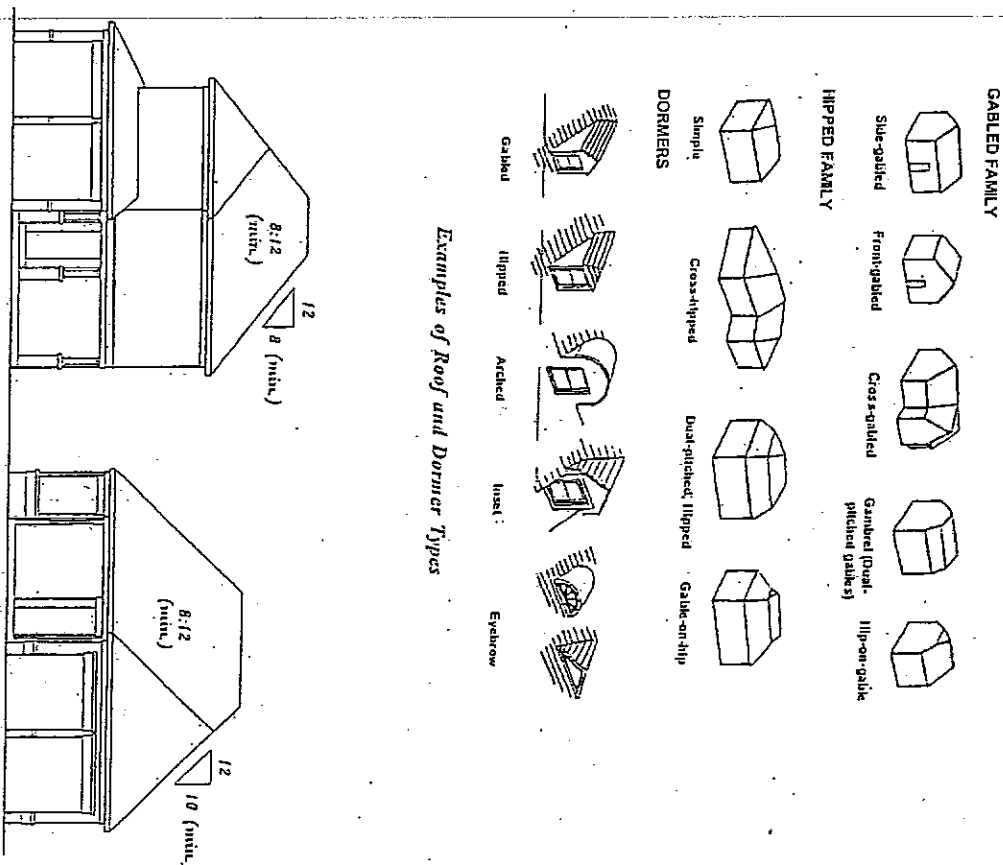
- As an alternative to vinyl or aluminum siding, the use of "Fibre-Cement Siding" (i.e. Hardi-Board) with either a shiplap profile or vertical board + batten profile may be used where appropriate to the architectural style.
- The use of simulated wood panelling, crezone and stuccato board is discouraged due to its maintenance problems.
- Exterior cladding on all dwelling elevations should be consistent with the cladding on the front elevation. Exceptions to this may be permitted where an upgraded stone or stucco façade is incorporated into the design. These features should return along the side walls a minimum of 3.0 metres from the front of the dwelling or to a logical stopping point such as an opening, downspout or change in plane.

- ii) Exterior Colours
 - Colour schemes and material selections should be carefully coordinated for visual harmony and for consistency with the architectural style of the dwelling.
 - Jarring contrasts in colour should be avoided.
 - Paint colours should be selected from the paint manufacturers' "historical colour collection".
 - Dwellings adjacent or directly opposite one another shall not have main wall cladding of the same colour. Identical main wall cladding colours shall be separated by a minimum of 3 dwellings.
 - The use of an accent colour for brick detailing such as lintels, bands or quoins, shall be complementary to the colour of the main façade brick.
 - The roof shingle colour should complement the colour of the primary wall cladding. The use of light coloured shingles, such as white or light grey, shall be avoided.
 - All flashings shall be prefinished or painted to match adjacent wall cladding colour or roof.

- iii) Architectural Detailing
 - Each dwelling design should include materials and architectural detailing characteristic to the style of the dwelling. These may include the following:
 - Brick : Soldier course banding or lintels, quoined corners, piers and corbelling (brick detailing should generally project 12 mm beyond the building face).
 - Precast : sills, lintels, keystones, imposts.
 - Stone : Stone accent features such as plinths or projections.
 - Stucco: Pre-finished, molded architectural details such as lintels, cornices, window surrounds, etc.
 - Wood trim : window and door casings, louvers, frieze boards, cornice and other moldings.
 - A continuous frieze board, cornice or soldier course banding is required on all elevations of the dwelling and garage underneath the roof soffit.

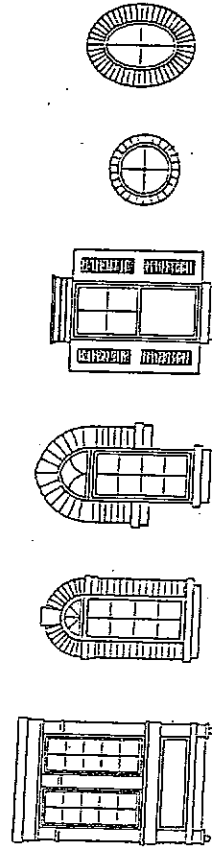
4.5 Roofs

- A variety of roof types and forms are encouraged consistent with the architectural style of the dwelling.
- Attention shall be paid to the relationships of adjacent roof forms to ensure appropriate transitions.
- The use of upgraded roofing materials is encouraged. This may include higher quality textured asphalt shingles (such as "Aristocrat", "Chateau" or "Renaissance" style shingles).
- Minimum main roof slopes should be 8:12 pitch. Pitches on side roof slopes, front facing hips, gables, cross gables and dormers should be greater where architecturally appropriate.
- Roof pitches for Bungalows should be steep 10:12 to 12:12 to facilitate massing compatibility with 2-storey dwellings.
- Certain architectural styles (i.e. Georgian or Greek Revival) may benefit from having roof slopes less than 8:12. The use of lower roof slopes will be at the discretion of the Control Architect on an individual basis and will be dependant upon the architectural style of the dwelling.
- Flat main roofs are not permitted, unless a component of a mansard roof.
- Roof overhangs should be a minimum of 12" (300 mm).
- All plumbing stacks, gas flues and roof vents should be located on the rear slope of the roof wherever possible and should be prefinished to match the roof colour.
- Where skylights are proposed, they should be located in low visibility areas such as on the rear or side slope of the roof and have a flat profile (e.g. Velux type).



4.6 Windows

- Window sizes should be generous and have proportions and details consistent with the architectural style of the dwelling, including integrated muntin bars where appropriate.
- All windows shall be thermally-sealed, maintenance-free, double-glazed casement or double-hung type. Slider type windows are discouraged.
- Vertical, rectangular window proportions are preferred to reflect traditional architectural styles. Other window shapes are encouraged as an accent.
- Main floor transom windows will be encouraged.
- Sills and lintels should be consistent with the architectural style of the dwelling.
- Bay windows should be used at appropriate locations and designed in a manner consistent with the architectural style of the dwelling.
- Ground level bay windows should include a foundation.
- At siding and stucco finishes, window and door apertures must have a 100 mm min. wide casing.
- Where shutters are used, they should be half the width of the window.



Example of Window Styles

4.8

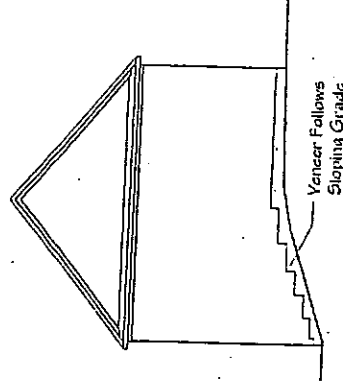
Adverse Grade Conditions

Where severely sloping grade conditions occur, the builder shall provide dwelling designs which suit the site. The following are suggested design approaches for reducing the height of elevated front entries and the impact of the large number of exterior steps they require :

- Integrate groups of steps into the front walkway over the length of the front yard.
- Provide a sunken foyer and internal steps up to the main living level.

4.7 Foundation Walls

- Exposed concrete foundation walls are to be avoided.
- Grading shall be coordinated with dwelling foundation design and construction to ensure that no more than 300 mm of foundation walls above grade is exposed.
- Where sloping finished grades occur, finished wall materials and foundations shall be stepped accordingly to minimize exposed foundation walls.

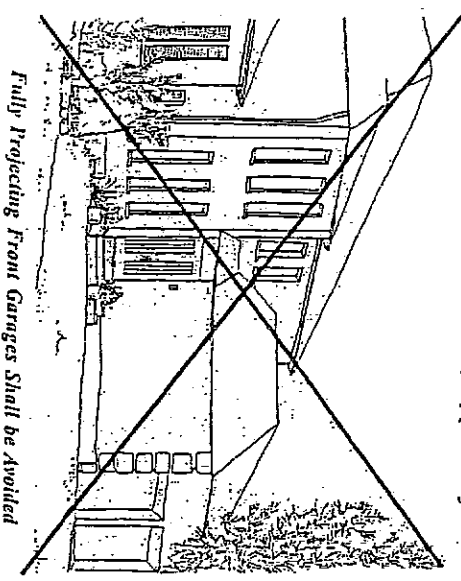


Foundation Wall at Sloping Grade

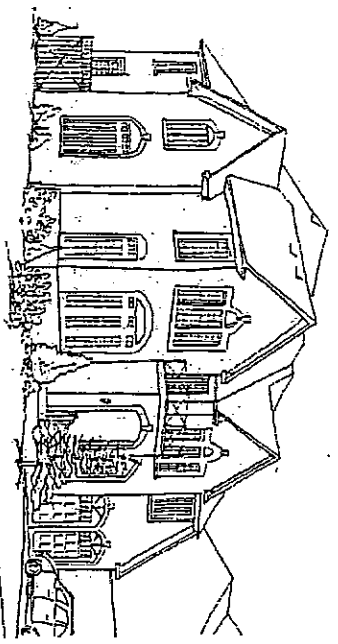
5. DESIGN GUIDELINES FOR GARAGES

5.1 General Requirements

- Attached garages shall be complementary in terms of character and quality to the principal dwelling.
- Garages facing the street should be flush with or recessed behind the front face of the dwelling or porch / portico face.
- Front projecting garages are generally not permitted. Notwithstanding this, garage projections up to 1.5m max. may be permitted by the Control Architect based upon the merits of the dwelling design. This projection allowance is considered an exception and should not be universally applied by homebuilders.

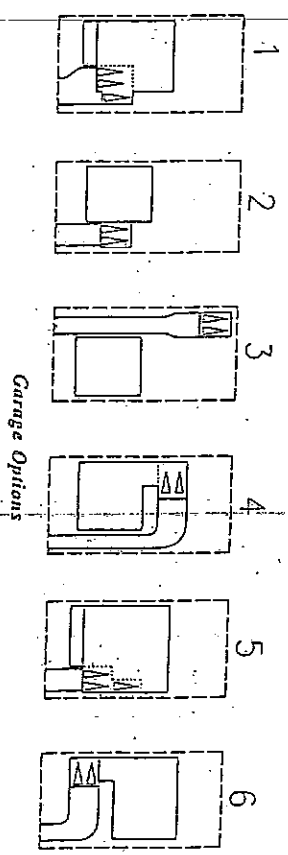


Fully Projecting Front Garages Shall be Avoided



Attached Garages Should be Integrated into the Massing Of the Dwelling and Located Behind the Front Wall Face

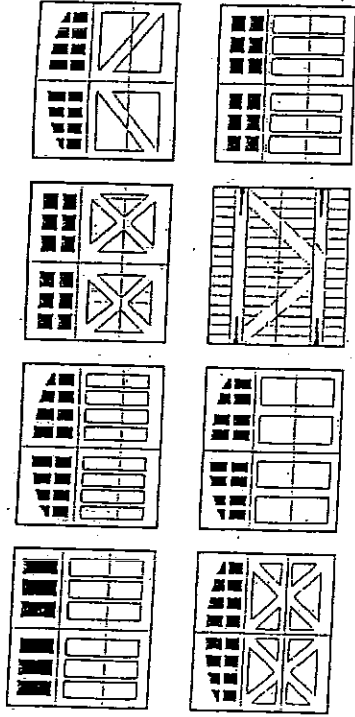
- Minimizing the appearance of the garage within the streetscape is a key requirement for all dwelling designs. This can be achieved in a number of different yet effective ways, such as:
 - 1) integrating the garage into the main massing of the house;
 - 2) locating an attached garage at the side of the house, set back behind the main front wall face;
 - 3) locating a detached garage in the rear yard;
 - 4) locating an attached garage in the rear yard;
 - 5) providing a tandem garage;
 - 6) providing a side facing garage in the front yard;



Garage Options

- Where the garage is visible to the street, the use of single-bay garage doors separated by a pier is preferred over 16'-0" wide double-bay doors.
- The second storey wall face located above the garage shall not be set back more than 2.5 m. Dwelling designs with the second storey wall face flush with the garage wall face below should be avoided unless sufficient detailing (i.e. windows, box-outs, articulation, an intermediate roof or other appropriate measures to break down the scale of this area) is provided in the design of the dwelling.

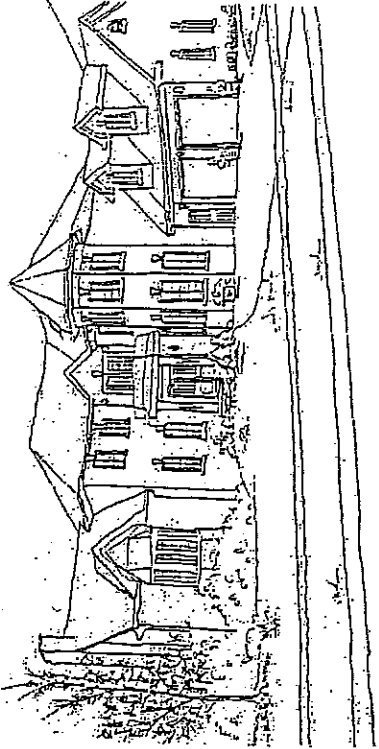
- The use of upgraded garage door styles will be encouraged. Garages should have paneled, sectional roll-up doors, with a variety of glazed top panels.



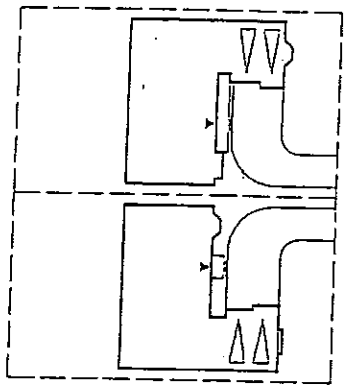
Variety of Upgraded Garage Door Styles

- Three-car garages are permitted on lots with frontages of 60 ft. or greater provided the face of the garage is articulated. This can be achieved by offsetting the outside bay by approx. 4'-0" (other solutions to provide articulation to the front face of the garage will be reviewed by the Control Architect on their merits).
- The maximum number of garage doors facing the street shall be 3.

- The treatment of the front wall of the garage facing the street shall exhibit design variety, ample fenestration and detailing consistent with that of the front facade of the habitable portion of the dwelling.
- Dwellings must be designed to allow for entry steps to project without interfering with vehicular access to the garage nearest to the house.
- Roofline variation above the garage doors should be provided through the use of habitable rooms, dormers and/or gables.
- The maximum number of garage bays is 3.



Side Facing Garage



Paired Side Facing Garages Creates Courtyard

5.2 Side Facing Garages

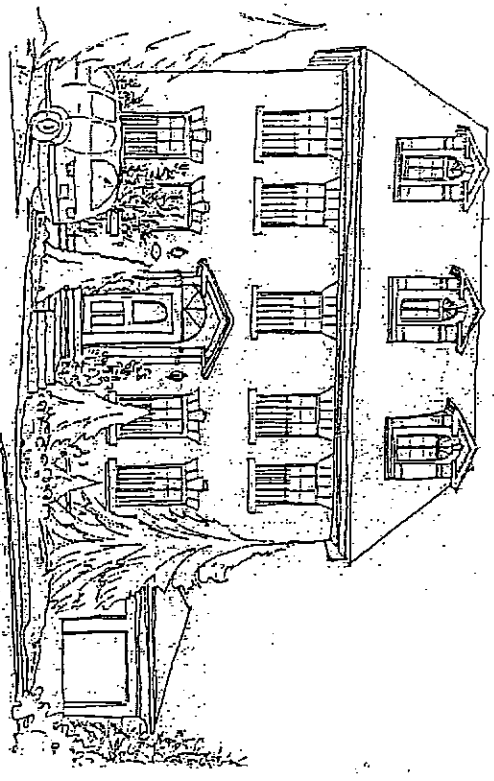
Side facing garages are generally discouraged due to the large projection of the garage from the main building facade. Side facing garages may be permitted on a limited basis for lots greater than 70 ft. in width subject to the following:

- Dwellings of this nature shall be paired with the garages located to the outside of the pair to create a courtyard effect between dwellings.

5.3 Rear Yard Garages

Rear yard garages are encouraged as an alternative means of minimizing the impact of the garage on the streetscape. Rear yard garages may be detached from the dwelling or attached to the rear wall of the dwelling. The following design criteria applies:

- Rear yard garages are to be of a complementary design quality (same cladding materials and colours) as the principal dwelling.
- Detached garages should be located close to the minimum setback permitted from the side and rear lot lines.
- Driveways accessing rear yard garages shall be kept to a single lane width. Nothing shall project into this driveway, such as steps, chimneys, wall projections or window wells to ensure a clear width of 3.0m.
- Detached garages, on corner lots shall be accessed from the flankage street and will be of increased design quality consistent with the main dwelling.



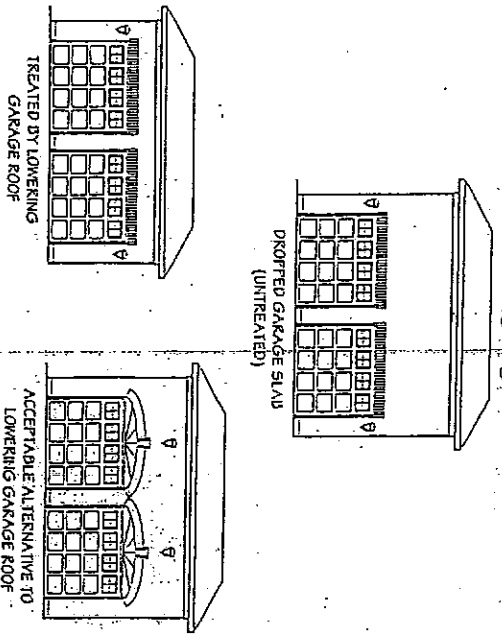
Example of Dwelling with Rear Yard Detached Garage

5.4

Criteria for Dropped Garage Conditions

Dropped garage conditions occur on rear-to-front sloping lots when additional risers at the front entry are required. This can create "top-heavy" garage massing by increasing the expanse between the top of the garage door opening and the underside of the soffit above.

- Suggested design treatments to reduce the visual impact of the taller garage include:
- increase the garage door height by 300 mm.
- lower the garage roof;
- add a decorative gable louvre or feature;
- provide additional detailing, such as masonry soldier coursing over lintels, or continuous brick banding.
- provide a window scaled to the dwelling, above the garage doors;
- provide wide profile arched lintels over the garage doors;
- locate light fixtures above garage doors.



Dropped Garage Condition

6. DESIGN CRITERIA FOR PRIORITY LOTS

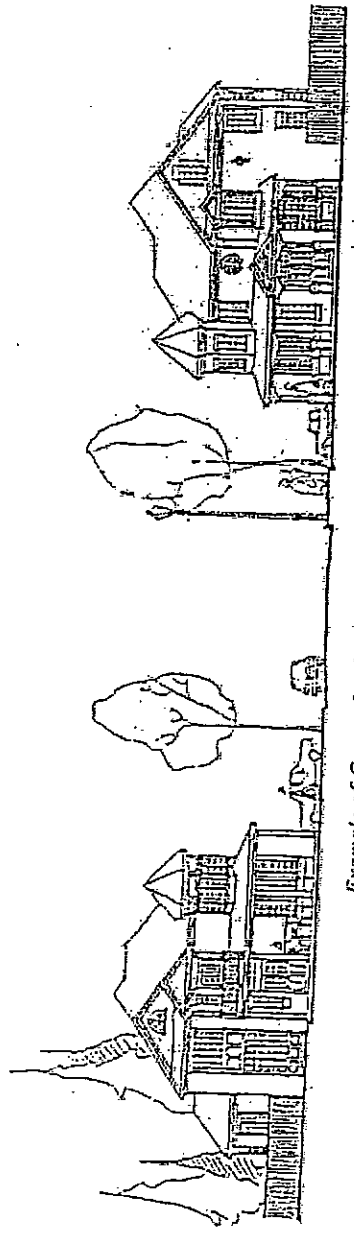
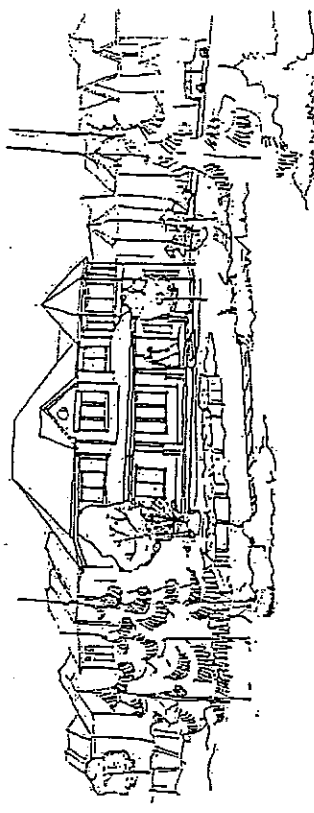
Lots which occur in visually prominent locations within the Neighbourhood are referred to as Priority Lots. Special design consideration is required for Priority Lot Dwellings as noted below:

6.1 Corner Lot Dwellings

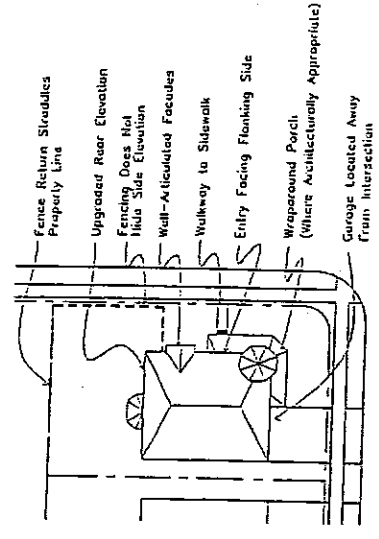
Corner Lot Dwellings have two façades fully exposed to the street. These dwellings play a significant role in setting the architectural image, character and quality of the street. The design of Corner Lot Dwellings should incorporate the following:

- Dwelling designs must be appropriate for corner lot locations.
- Dwelling designs intended for internal lots will not be permitted.
- Both street frontages for corner lot dwellings shall have equivalent levels of architectural design and detail with attention given to the dwelling's massing, roof lines, apertures, materials and details.
- Enhanced rear elevation detailing and windows, equivalent to the street facing elevations, is also required.
- The main entry to the dwelling is encouraged on the long elevation of the dwelling facing the flanking street.
- Main entries facing the front lot line or shorter side of the lot are discouraged. Where the dwelling design has the main entrance within the building face at the shorter side of the lot, the design of the flanking face shall include a secondary entry, projecting bay or other appropriate feature.

- The design of corner dwellings should incorporate features that provide emphasis to the corner of the structure and its side elevation, such as:
 - entry portico or porch on the long side of the dwelling.
 - well proportioned apertures for doors and windows, located to create well balanced elevations.
 - wall projections along the flanking wall face such as bay windows, tower features, wrap-around porches, etc.
 - gables, dormers, eyebrow window or other appropriate elements to enhance the roof form.
- A privacy fence shall be provided to enclose the rear yard of corner lot dwellings.



Example of Corner Lot Dwellings



Corner Lot Dwelling - Plan View

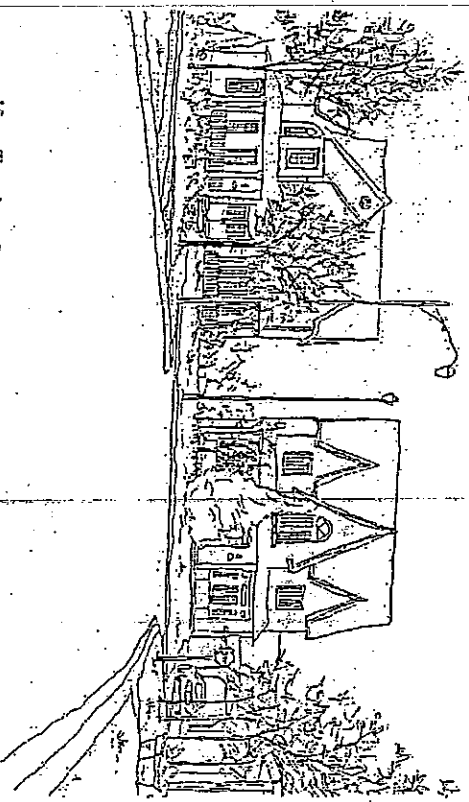
6.2 View Terminus Dwellings

View Terminus Dwellings typically occur where one road terminates at right angles to another. These dwellings terminate an axial view corridor and should receive enhanced architectural design and landscaping treatment. Guidelines for View Terminus Dwellings are as follows:

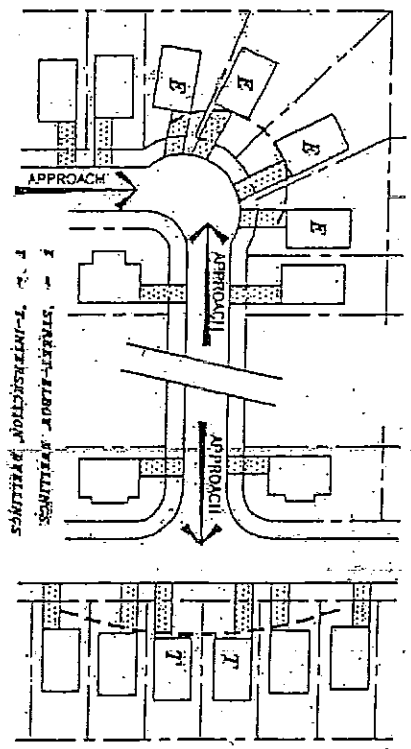
- Where lot depths permit, View Terminus Dwellings should have a greater front yard setback than adjacent dwellings.
- Driveways for paired View Terminus Dwellings should be located to the outside of the lots to provide opportunities for increased landscaped treatment, reduce the visual impact of the garages on the axial view and create a stronger architectural image.
- View Terminus Dwellings should have enhanced design or architectural detailing, giving them greater visual interest.

Dwellings on the outside lots of curved streets and street elbows are also considered View Terminus Dwellings and should have design enhancements to accent the outside street edge, as follows:

- Provide higher quality of architectural design and greater front yard setbacks, where feasible, than for adjacent dwellings.
- Locate driveways to the outside of paired lots, to allow for enhanced front yard landscaping opportunities.
- Where the lots are pie-shaped, utilize the opportunity to locate detached garages within the wider portion of the lot, set well back from the street.



View Terminus Dwellings should be designed to provide visual interest



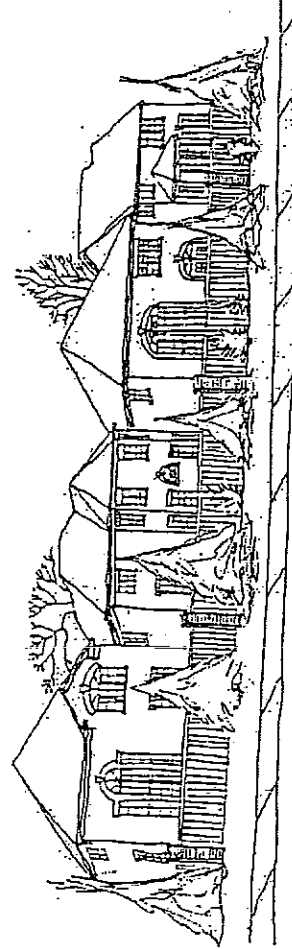
VIEW TERMINUS DWELLINGS

6.3 Upgraded Rear & Side Yard Architecture

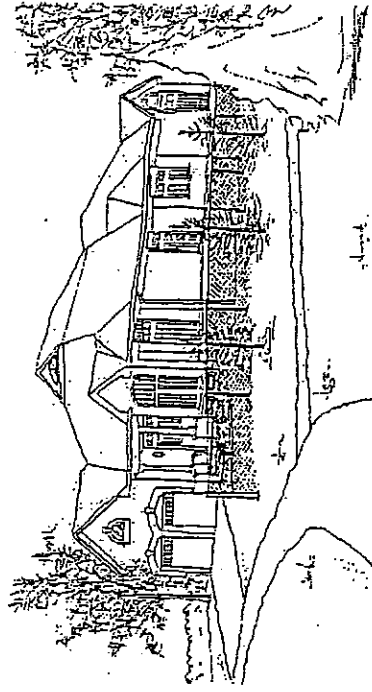
Where a dwelling's side or rear elevations are exposed to public view, they require enhanced design treatment, having detail and quality consistent with the street-facing elevation.

- Applicable enhancement situations include the following:
 - Dwellings backing or flanking onto open space, public walkways, storm water management ponds, parks, or other public uses.
 - Reverse frontage lots backing or flanking onto a public road.
 - Dwellings on curved streets where stepped setbacks leave sidewalls exposed to public view.

- Applicable enhancements on the exposed elevations include the following:
 - Bay windows or other additional fenestration, and enhancement of windows with shutters, muntin bars, frieze board, precast or brick detailing.
 - Gables, dormers or eye-brow window within the roof.
- Where the exposed elevations occur adjacent areas of limited public visibility, such as a heavily treed woodlot, the level of architectural enhancement may be reduced.



Upgraded Rear Architecture (backing onto publicly exposed areas)

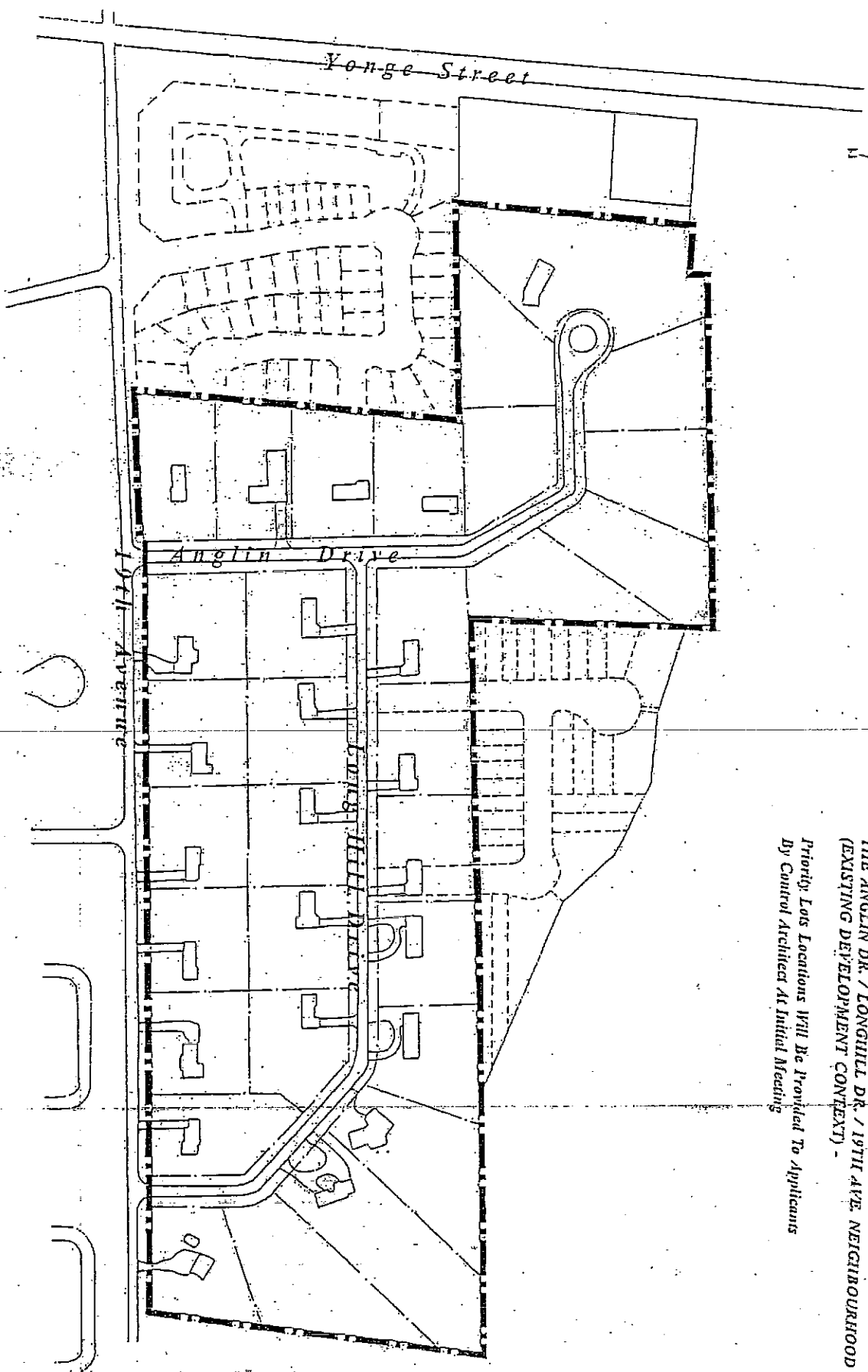


Upgraded Side Architecture (flanking onto park / open space)

Schedule A

THE ANGLIN DR. / LONGHILL DR. / 19TH AVE. NEIGHBOURHOOD
(EXISTING DEVELOPMENT CONTEXT) -

Priority Lots Locations Will Be Provided To Applicants
By Control Architect At Initial Meeting



Appendix A – ARCHITECTURAL DESIGN REVIEW CHECKLIST

The following is a list of items that may be reviewed by Control Architect in the development of lots:

1. Site Plan

- House type, model / elevation number
- Driveway layout and surface material
- Setbacks; compatibility with setbacks of adjacent dwellings
- Sidewalks / walkways
- Existing trees with existing and proposed grades
- Lot Grading and drainage
- Service lead-ins (storm, sanitary, water, hydro, gas and telephone)
- Hydro transformer, meters, telephone, cable switch gears
- Fencing (Corner lot, screen and acoustic), wall locations, height, materials and colours

2. Building Design, Colour And Materials Selection Chart

- Elevations (front, side and rear elevations)
- Architectural style
- Building massing relative to adjacent dwellings
- Location of garage
- Windows, doors, skylights, garage doors and details
- Materials (including samples)
- Deck and walkout plans and details
- All mechanical equipment, vent stacks etc on elevations or doors (location, size and colour)
- Upgraded elevations.

Appendix B – Typical Exterior Material and Colour Schedule

	Brick	Stucco	Stone	Siding	Roof Shingles	Alum. Siding (soft/eyes downspout)/ Frieze Board	Entry Door Paint	Garage Door Paint	Trim Paint (Sills/ Cornices/ Frames, etc.)	Structs	Windows	Exterior Railings	Flashing/ Roof Vents	Median Trim
Main Entrance														
1st Entrance														
2nd Entrance														

GENERAL NOTES:

- All exterior colour selections are subject to Architectural Control Approval in compliance with the Architectural Design Guidelines.
- All roof vents & flashings to be painted to match roof colour.
- Exterior colour samples to be provided. Discuss with Control Architect prior to submission.

Appendix C – FEE SCHEDULE

- A privately administered architectural review process will be conducted by John G. Williams Limited, Architect to certify compliance with the objectives and requirements of the Architectural Design Guidelines. This will take the form of:
- An initial meeting with the applicant (builder) and the house designer.
 - Review of preliminary design plans by control architect.
 - Comments provided to applicant.
 - Submission of final working drawings, site plan and exterior colour schedule for stamped approval by control architect.
 - Plans bearing the stamp of the Control Architect must be provided to the Town of Richmond Hill Building Department as part of the application for building permit.
 - The Control Architect will conduct periodic site visits to monitor for compliance with the Guidelines. Deficiencies will be reported to the builder and the Town with suggestions of appropriate solutions.

FEE:

- \$500.00 per dwelling unit

NOTE:

- Fees for implementation of Architectural Control Process will be billed directly to the applicant / owner and must be paid prior to release of stamped plans by control architect.
- All fees are subject to the addition of direct disbursements (at cost) and GST.
- Where extra meetings or work by the Control Architect is made necessary due to non-compliance with the Architectural Design Guidelines, this shall be billed at the rate of \$100.00 per hour. Applicants will be notified of this prior to final approval of plans.

