

STAFREP/SRP98094	
TOWN OF RICHMOND HILL PLANNING AND DEVELOPMENT DEPARTMENT	SPECIAL PLANNING AND DEVELOPMENT COMMITTEE MEETING
PREPARED BY: <u>LYNN POOLE</u>	JUNE 9, 1998
TELEPHONE NO.: <u>905-771-8910</u>	STAFF REPORT: SRP.98.094
	FILE NO.: D00-NU-PU-SR

SUBJECT:

Infill Residential Study for the Puccini Drive Neighbourhood.

RECOMMENDATION:

1. That Planning and Development Committee receive Staff Report SRP.98.094 and recommend that Council approve the following key principles, prepared by The Planning Partnership, to guide future infill development in the Puccini Drive Neighbourhood:
 - a) the edges of the neighbourhood should be developed with medium density housing;
 - b) the lot frontages on existing streets should be not less than 15 metres (50 feet);
 - c) traffic calming measures should be implemented on Puccini Drive; and,
 - d) natural features should be protected.
2. That the Demonstration Plan, prepared by The Planning Partnership, and contained in Staff Report SRP.98.094, be endorsed as an illustration of one alternative concept for achieving redevelopment in accordance with the key principles identified in Recommendation #1 above; and,
3. That the Design Guidelines prepared by The Planning Partnership and contained in Appendix "B" to Staff Report SRP.98.094 be endorsed and used to guide the detailed planning and design of infill development in the Puccini Drive Neighbourhood and the evaluation of future development applications.

BACKGROUND:

Official Plan Amendment 129 designates various infill areas within the existing residential areas of the North Urban Area (shown on Map 1). The infill policies require that Council approve comprehensive concept plans prior to the development and redevelopment of these areas. Accordingly, in the fall of 1997, The Planning Partnership was retained to prepare Infill Concept Plans for the Puccini Drive Neighbourhood. After an extensive public consultation process which included three residents meetings, use of the workshop format and mail-in surveys, the Infill Study is now completed.

The report by The Planning Partnership, including a Demonstration Plan for Infill Development in the neighbourhood is attached in Appendix "A". The work by the Planning Partnership meets the requirement in OPA 129 for the preparation of a comprehensive infill plan and, accordingly, applications for zoning amendments, plans of subdivision and severances can be processed.

It should be noted that the Demonstration Plan (shown on Map 2), illustrates only one possible scenario for achieving development which meets the general planning principles and design guidelines set out in the consultant's report. Depending upon the applications submitted, there may be alternative lotting patterns or site layouts which could be designed which meet the intent and content of the guidelines. The Demonstration Plan is therefore not intended to serve as a final development concept or preclude concepts and design solutions that may be proposed as part of an application for development.

SUMMARY OF DEVELOPMENT CONCEPT

During the residents' meetings, there were a number of discussions regarding the form and size of lots which would be appropriate in the Puccini Drive area. While some residents felt strongly that the large lot character of the neighbourhood should be maintained, a majority of residents indicated that they have been waiting for some time to be able to have the opportunity to develop their existing lots. The consultants undertook a design analysis of the streetscape which would result from the development of 75 ft., 60 ft. and 50 ft. frontages. The results are illustrated on Figure 3 in the report contained in Appendix "A". The conclusions reached by the consultants were that given the position of existing residents and their desire to create infill lots which respond to market demands, the streetscape resulting from 15 metre (50ft) lots is not substantially different from that of 18 metre (60 ft.) lots.

On this basis, the Demonstration Plan shows 15 metre (50 foot) lots for properties fronting on Puccini Drive for the length of the Study Area, wherever site conditions permit. Wide shallow residential lots with lot frontages of 12 metres (40 feet) are recommended on the rear lots. Access is provided with new 18.5 (60 foot) municipal roads.











INFILL - RESIDENTIAL STUDY AREAS

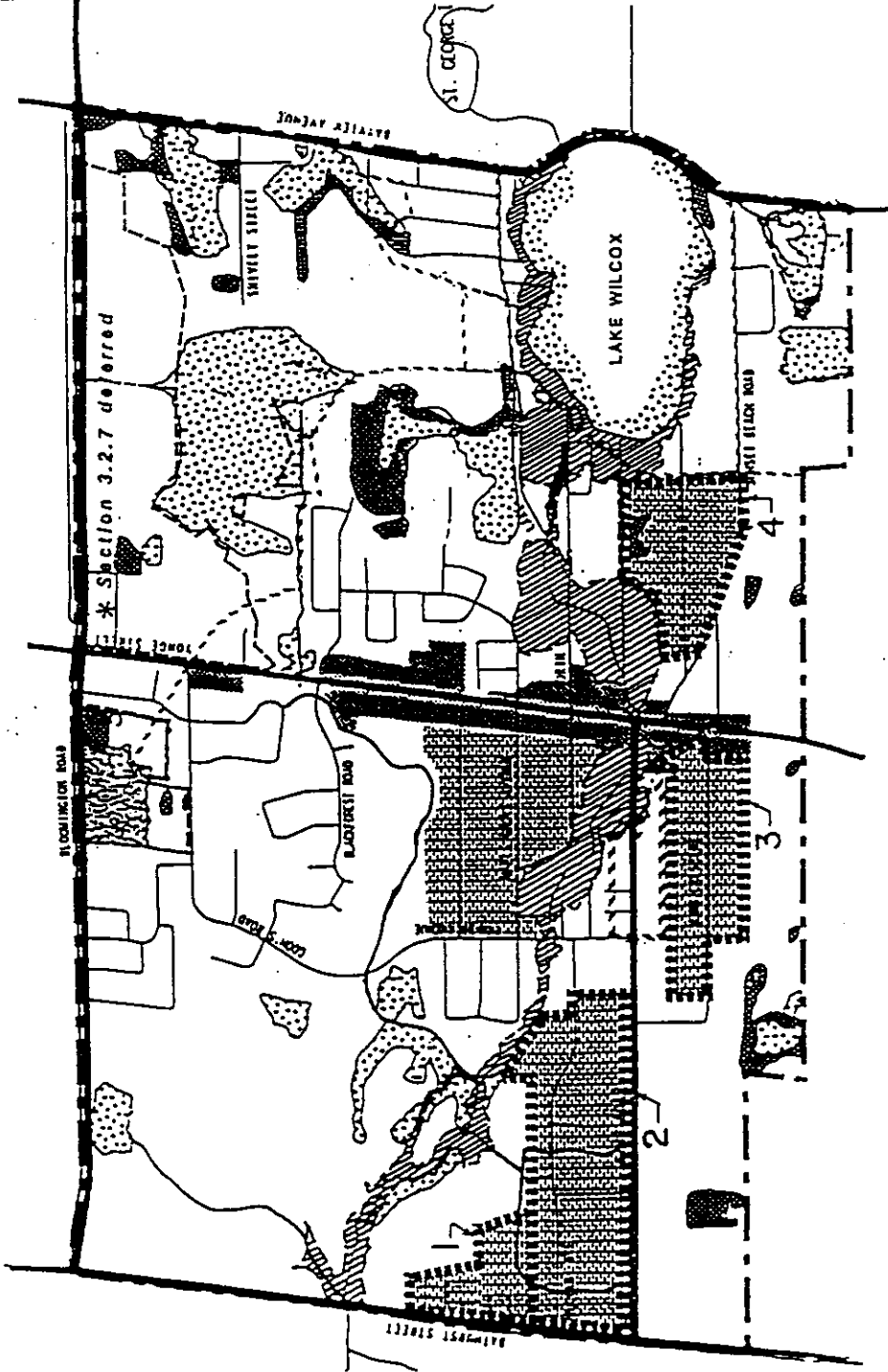
LAKE WILCOX - OAK RIDGES

Town of Richmond Hill
Schedule B



Community Structure

Legend

-  Community Core
-  King Rd. Commercial Area
-  Infill
-  Development Area
-  Hazard Land
-  Environmental Protection Area 1
-  Environmental Protection Area 2
-  Arterial Roads Under Provincial or Regional Jurisdiction
-  Land Form Conservation (Subject to Section 3.2.7)
-  OPA I29 Boundary

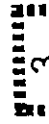



STUDY AREAS

-  1
-  2

BATHURST STREET NEIGHBOURHOOD

PUCCINI DRIVE NEIGHBOURHOOD

-  3
-  4

BOND CRESCENT NEIGHBOURHOOD

DOUGLAS ROAD NEIGHBOURHOOD

3000003

JJ/MB

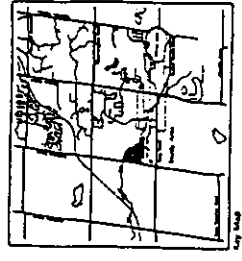
SRP

MAP 1

BLOCKS 1 & 2

FILE NO. D10-71-B-BA

TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT



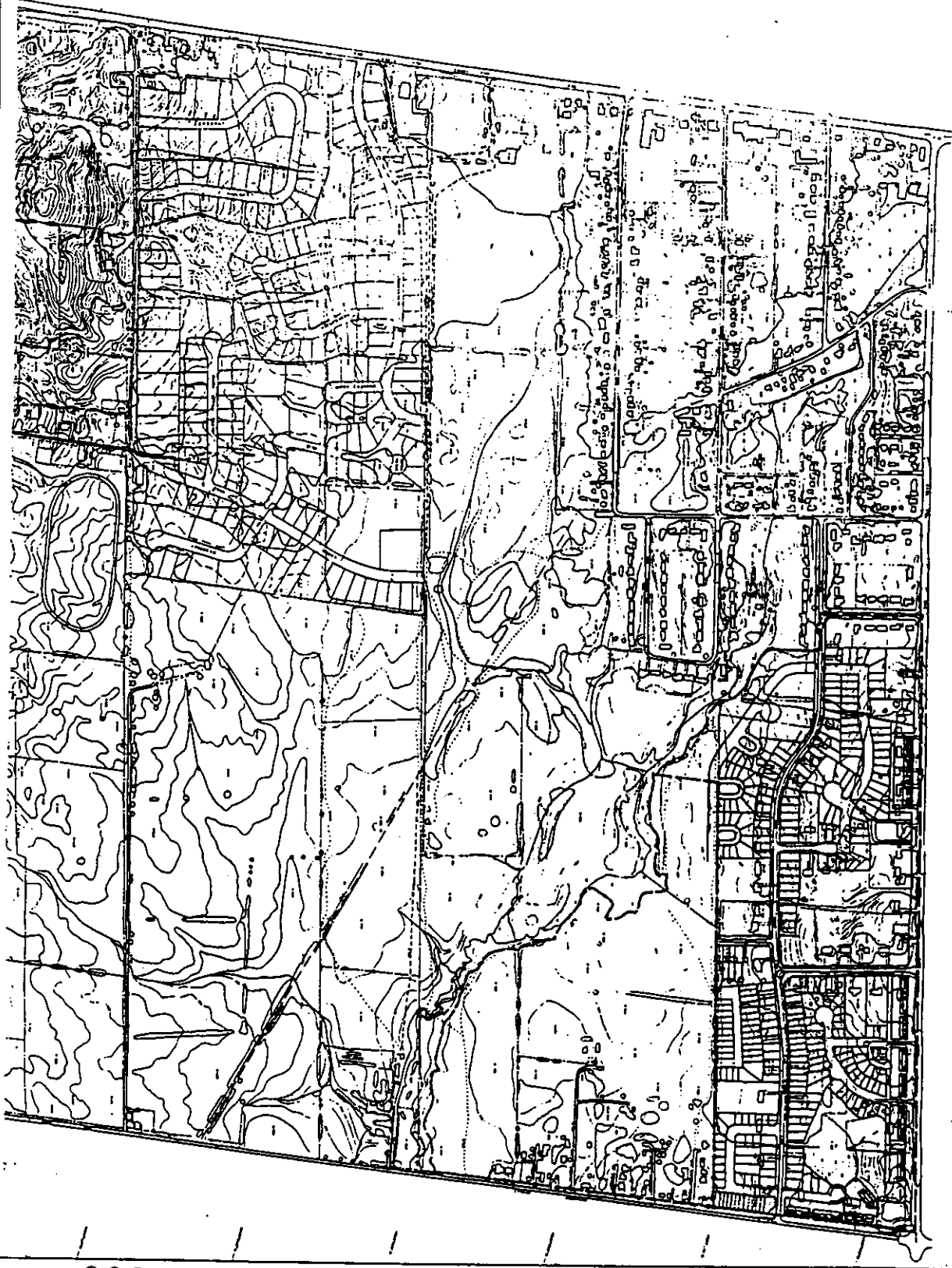
Demonstration Plan
Area 1
Puccini West

Town of Richmond Hill

Residential Infill
Bathurst Street
Neighbourhood

The Planning Department

Map 2



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Loop roads are recommended wherever possible. Drainage courses and areas with steep slopes have been protected in open space. Townhouses are illustrated in some locations at the edges of the neighbourhoods along Bathurst Street and King Side Road. Due to the restricted access onto arterial roads, townhouses are to be developed with rear access laneway systems or service roads abutting the arterial.

MAJOR ISSUES

This section contains a description of how the Infill Plan addresses issues raised at residents' meetings.

Maintaining Character While Realizing Redevelopment Potential

The Planning Partnership identified three issues associated with neighbourhood character and redevelopment which came from their extensive discussions with and surveys of residents:

- the need to establish a minimum lot width for properties fronting Puccini Drive to facilitate redevelopment;
- appropriateness of medium density residential uses along Bathurst Street and King Side Road; and,
- the need for a minimum lot depth and lot width for rear lot development.

Staff support the consultant's conclusion that the determination of preservation of community character is somewhat subjective and that urban design including house form, minimum lot frontages and front yard depth can be effective in maintaining the residential character of the neighbourhood. They noted that the wishes of the residents need to be taken into account, and, that in the absence of a clear public interest to protect the existing large lot character of the neighbourhood, it is the consensus of the community that will ultimately determine the form and pattern of infill development. The preparation of an infill plan must involve the property owners, and, in the end, be supported by the people the plan will affect.

Staff support the key principles and design guidelines including the recommendations for 15 metre (50 foot) lots for properties fronting Puccini Drive with 12 metre (40 foot) lot sizes for rear lot infill development.

Staff support the recommendation that the edges of the neighbourhood on arterials be used as an acoustic buffer and an opportunity to increase density in the area. Townhouses are an appropriate form of development on these high traffic roads provided that access can be accommodated in rear lanes or service roads abutting the arterial road.

Traffic Calming on Puccini Drive

The consultants noted that the Puccini Drive/Poplar Drive route is seen by residents as a speedway. As well, the consultants conclude that with new development in the area, traffic on Puccini Drive will increase and the potential exists that residents will use Puccini Drive to by-pass the King Road/Bathurst Street intersection. Accordingly, a traffic circle is proposed at the intersection of Toscannini Road and Puccini Drive to moderate speed.

The Engineering and Public Works Department advises that implementation of a traffic circle requires additional land outside of the right-of-way in order to provide more space for cars and trucks to safely travel around the turning circle. The implementation of the turning circle therefore, is dependent on the acquisition of land from property owners that front onto the Puccini Drive/Toscannini Road intersection during the development process. In addition, comments from the Fire and Transit Sections will be required in respect to the location, geometrics and implications of turning circles on this street.

Application for Development at the northeast corner of Puccini Drive and Bathurst Street

An application has been filed (see attached) for draft plan approval of a 53 unit subdivision comprising 13 townhouse units and 40 single detached homes on 15 metre (50 foot) and 13.5 metre (44 foot) lots (19T 98004). The Planning Partnership has reviewed the application and comments that the subdivision design, as proposed, does not relate well to the Puccini Drive neighbourhood. Issues identified by Town staff include, the proposed subdivision design, connections to the Bathurst Street neighbourhood to the north and the Puccini Drive neighbourhood to the east and the proposed piping of an EPA 2 intermittent drainage course. The application will be circulated and reviewed and discussions held with the property owner prior to a Council Public Meeting.

Implementation

During the residents meetings, there were questions on implementation of infill development including the need for adjacent property owners to cooperate to achieve a lotting pattern consistent with the lot size recommendations in the Infill Study and the process for proceeding with development approvals, once the Infill Study is approved by Council. There were also questions about the costs associated with development applications such as development charges and parks cash-in-lieu payments and the implications of infill development on property taxes.

Once the comprehensive Infill Plan is approved, applications for zoning amendments, plans of subdivision and consents to sever can be processed. Infill development in the Puccini Drive neighbourhood will proceed on an application by application basis, in order to accommodate the specific requirements of each group of property owners, or individual owners in the case of severances.

Zoning By-law 1275 applies to lands in the Puccini neighbourhood and dates from 1976. It reflects the original large lot development pattern of 1 and 2 acre lots on private services. An amendment to the North Urban Area Zoning By-law 313-96, adopted as part of the OPA 129 process, will be required for property owners who proceed either by severance or plan of subdivision.

Owners are encouraged wherever appropriate to co-ordinate and file applications that are comprehensive. All applications are to generally conform to the key principles and Design Guidelines for the Puccini Drive neighbourhood, contained in Staff Report SRP.98.094 as well as address Regional standards and requirements, where applicable.

The zoning standards for infill development recommended by The Planning Partnership include:

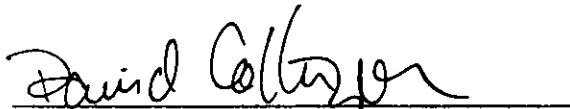
- minimum frontage on Puccini Drive, Verdi Road and Toscannini Road is 15 metres (50 feet) with a 1.5 metre (5 foot) sideyard setback and a 4.5 metre (15 foot) front yard setback to the main wall of the dwelling;
- wide shallow lots are to have a minimum 25 metre (82 feet) depth and minimum 12 metre (40 foot) frontage;
- townhouses are to have a minimum 6 metre (20 foot) frontage; and,
- the principles of small lot development such as the front face of the garage is not to extend beyond the front face of the living area should be implemented.

CONCLUSIONS:

The development of the Infill Study, attached in Appendix "A" was based on extensive consultation and input from interested residents. The process involved discussions between residents as to their vision of the future of their neighbourhood. It was clear that some residents would like to proceed with infill plans in the short term while others wish to retain the large lots on which their homes are situated, while maintaining the opportunity to redevelop in the future.

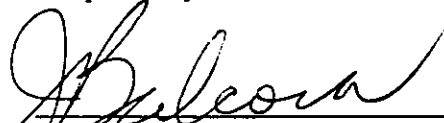
The principles, Demonstration Plan and Design Guidelines for the Puccini Drive neighbourhood, prepared by the Planning Partnership, generally address the concerns of the residents and reflect the general nature of the opinions and views expressed at the resident meetings. It is recommended that Planning and Development Committee recommend to Council, the approval of the principles, Demonstration Plan and Design Guidelines attached in Appendices "A" and "B" in order to satisfy the requirements of OPA 129 and further, to provide the basis for the review of development applications within the Puccini Drive area.

Reviewed by:



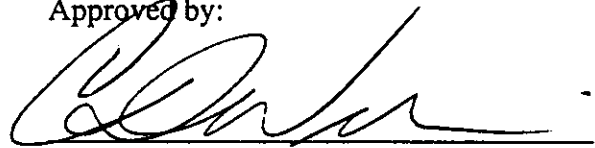
David Collinson,
Director of Policy

Respectfully submitted:



Janet E. Babcock, M.Sc., MCIP,
Commissioner of Planning and Development

Approved by:



C.D. Weldon,
Chief Administrative Officer

DATE: June 3, 1998

LP:DC:JEB:CDW:bf

LP rf

Attachments

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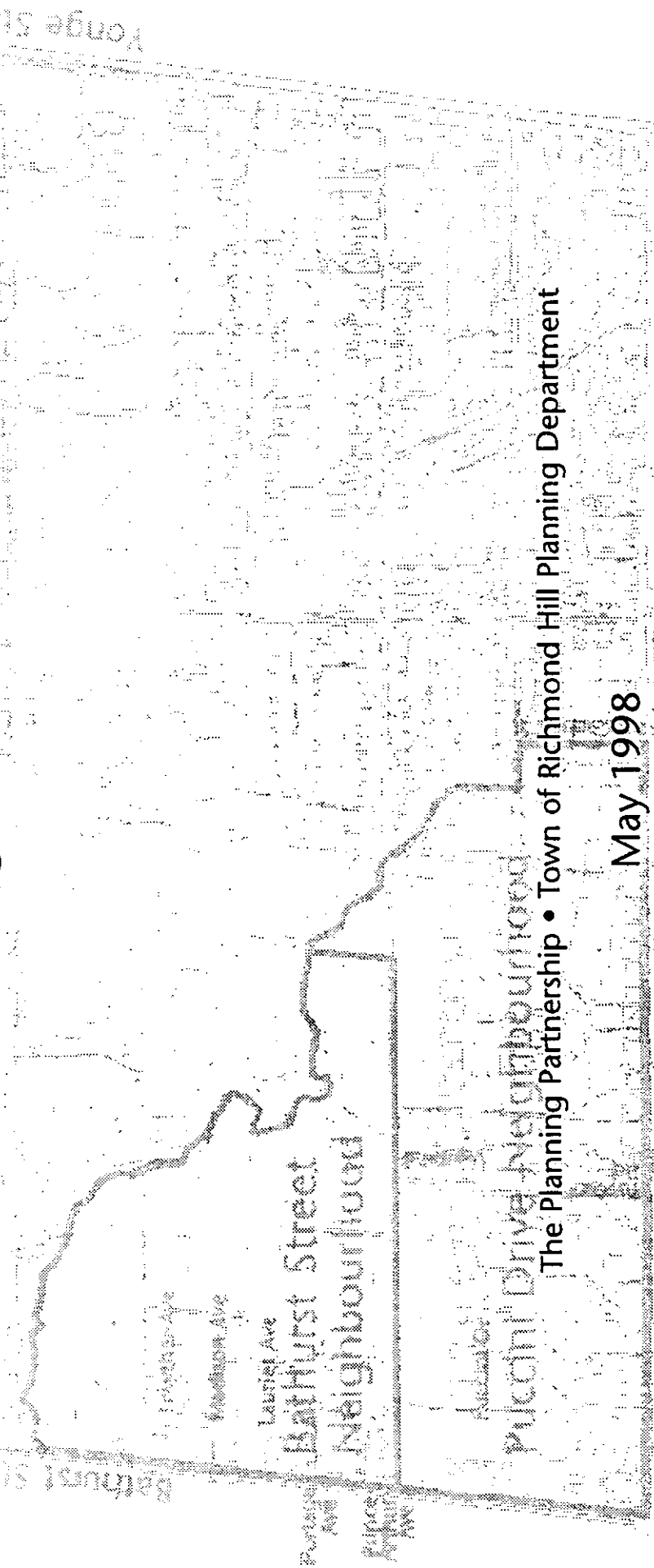
APPENDIX CONTENTS

“A” Final Report by the Planning Partnership, May 1998 - ATTACHED

“B” Design Guidelines for the Puccini Drive Neighbourhood - ATTACHED

Town of Richmond Hill

Bathurst and Puccini Neighbourhoods Infill Development



The Planning Partnership • Town of Richmond Hill Planning Department

May 1998

King Side Road

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TOWN OF RICHMOND HILL PLANNING DEPARTMENT DEVELOPMENT
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1.0 INTRODUCTION

The Town of Richmond Hill retained The Planning Partnership to provide the technical basis on which plans to redevelop two neighbourhoods in Richmond Hill would be evaluated by Council and Staff. The two neighbourhoods are the Puccini Drive neighbourhood and the Bathurst Street neighbourhood. The extent of both of these neighbourhoods is shown on Figure 1.

2.0 POLICY BASIS

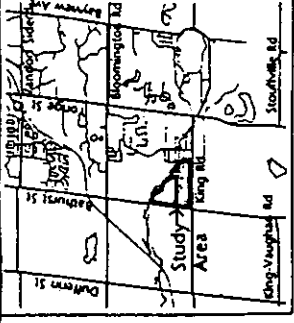
Official Plan Amendment Number 129 (OPA 129) was prepared by the Town of Richmond Hill to respond to development pressures in the north end of the Town. The intent of OPA 129 was to manage change in this area so that the impacts of that change on the natural environment are minimized. As a result, the OPA contains a number of policies that put the 'environment first'. These policies are intended to ensure that the integrity of the natural environment of the Oak Ridges Moraine is maintained and enhanced as development occurs.

In addition to identifying areas where new development on greenfield sites would occur, OPA 129 also encourages infilling and redevelopment within existing built-up areas. The Puccini and Bathurst Street neighbourhoods have been identified as such areas by OPA 129. At the present time, much of this area is the site of single detached homes on large lots on full municipal services. Many of the lots are deep, meaning that new streets could easily be developed at the rear of these lots to access the new development without any change occurring at the front of the lot.

OPA 129 requires that the predominant housing form in infill areas be single detached dwellings. Higher density housing in the form of townhousing and walk-up apartments is permitted on arterial roads such as King Road and Bathurst Street.

Given that there will be continuing pressure to redevelop within the existing built-up areas, OPA 129 recognizes that there is a need to prepare comprehensive plans for the infill areas to provide the basis for future planning decisions. This comprehensive plan is required to ensure that roads and services are developed in an efficient manner and to ensure that development occurs in appropriate locations. Once the comprehensive plan is in place, applications for Plan of Subdivision or consent to sever can be processed, provided, provided they generally to conform to the plan for the neighbourhood.

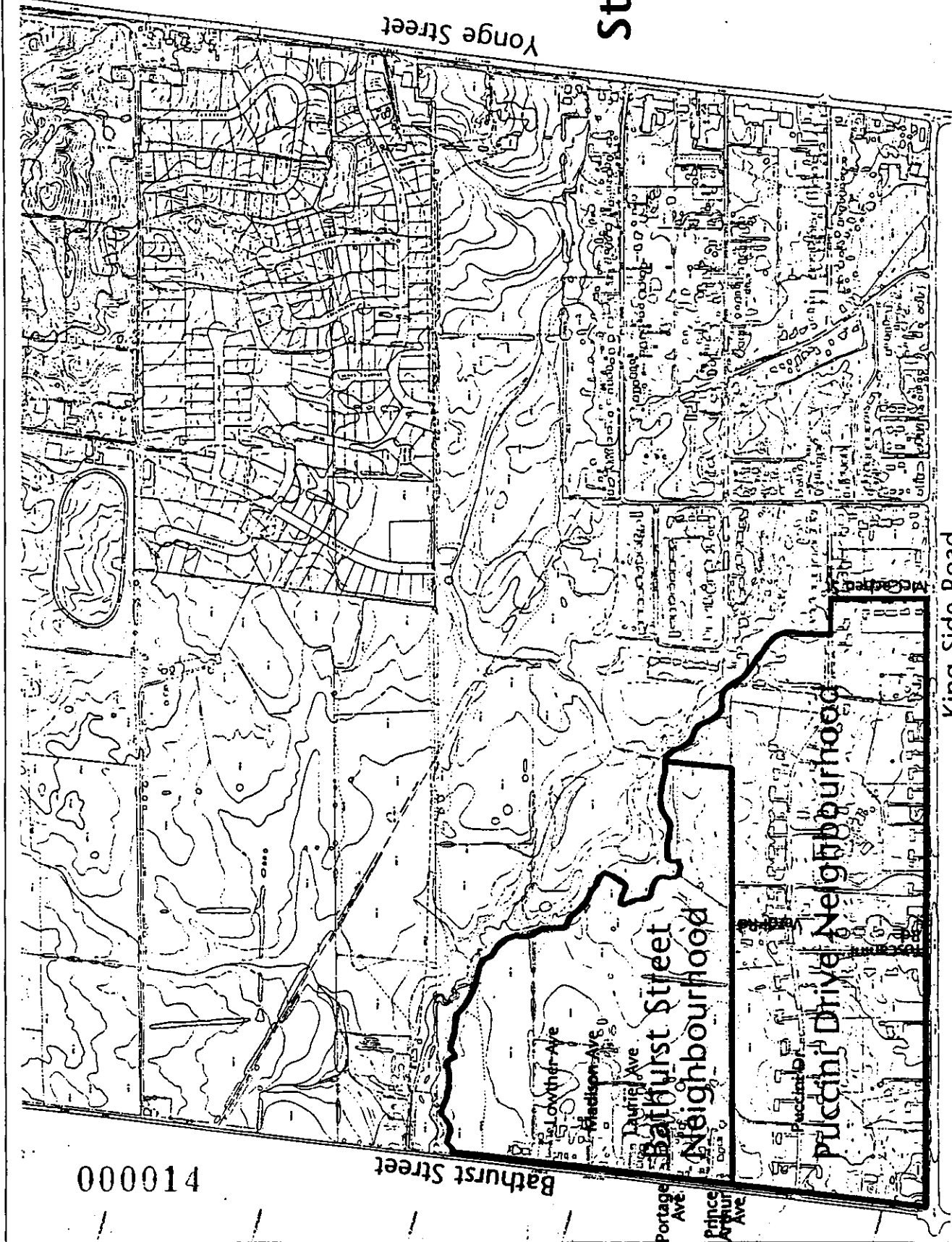
Although infill development is encouraged by OPA 129, it is also stated, in Section 4.1(a) that *"the density, character and form of new development will be such that the desirable aspects of the existing community are preserved."* The determination of what is 'desirable' is subjective. To some, it may mean the nature of the house form, to others it may mean lot frontage and front yard depth. These are important considerations in neighbourhoods where a certain uniqueness exists and/or where there is a public interest to protect the



Key Map

Study Areas

Figure 1



King Side Road

410000

area's character. This public interest is sometimes expressed by identifying an area as a 'Conservation District' in accordance with the Ontario Heritage Act. None of the infill areas has been identified as a 'Conservation District.'

The wishes of the residents also need to be taken into account in determining what elements of a community's character needs to be preserved. In the absence of a clear public interest to protect the character of a neighbourhood, it is the consensus of the community that carries the most weight. As a result, the preparation of an infill plan for a neighbourhood must involve residents and in the end, be supported by the people the infill plan will affect.

3.0 THE PUBLIC PROCESS

Given the nature of the proposed development, an ambitious program of consulting the public in the areas affected was initiated by the Town in September 1997. This public participation program was designed to ensure that all of the stakeholders were given an opportunity to comment on the concepts and design guidelines for the area. The public consultation mechanisms and the results are described below.

First Workshop was held in September 1997

The first workshop was conducted on September 23, 1997 for Puccini Drive residents and September 29, 1997 for Bathurst Street residents to discuss general issues and concerns regarding infill development. Both workshops were well attended. It was concluded at this workshop that a survey would be distributed requesting specific information on preferences for development.

Survey of residents sent out in October 1997

A survey was subsequently sent to all residents in both study areas asking for a yes/no answer to three questions:

1. Are you in favour of infill development in your neighbourhood?
2. Would you like your property to be part of a plan for infill development?
3. If "yes", do you intend to stay in your present house?

In the Bathurst Street infill area, all respondents were in favour of redevelopment except one.

In the Puccini Drive neighbourhood, most residents west of Toscanini Road indicated that they were in favour of infill development. The two opposed to infill were located on the north side of Puccini Drive, one (64 Puccini Drive) is in a critical location for an infill plan as it is directly on the route of a proposed north-south road extension which is part of the approved draft plan of the area to the north.

East of Toscanini Road, many of the residents were opposed to infill development. Four of the seven respondents on the north side of Puccini Drive were opposed to infill. There is a road on an approved draft plan of subdivision immediately adjoining the north side of their properties offering them an opportunity to develop the north end of their property. This area does not have enough depth to accommodate a tier of development between Puccini and the proposed new road.

On the south side of Puccini Drive, opponents of redevelopment are scattered throughout the neighbourhood. Two on Aida Place are key because of the need to extend a road connecting with this cul-de-sac behind existing houses to accommodate infill development.

Figure 2 illustrates all of those in favour of infill development at the time the questionnaire was circulated.

Second Workshop was held in January 1998

A second workshop was conducted on January 29, 1998 to review options for infill development. Residents were divided into three groups:

- Bathurst Street neighbourhood
- Puccini Street neighbourhood west of Toscanini, and,
- Puccini Street neighbourhood east of Toscanini.

The discussion with the Bathurst Street residents focused on:

- use of the conservation lands for recreation;
- need for trails;
- appropriateness of medium density residential uses on Bathurst Street;
- need to establish a minimum lot width, possibly of 40 feet to enable redevelopment; and
- need to maintain existing trees.

RICHMOND HILL INFILL SURVEY RESPONSES

THE PLANNING PARTNERSHIP

FEB. 3/98



* SALVA PROPERTIES ASSUMED PRO DEVELOPMENT

QUESTIONS:

1. ARE YOU IN FAVOUR OF INFILL DEVELOPMENT IN YOUR NEIGHBOURHOOD? YES ✓ NO X
2. WOULD YOU LIKE YOUR PROPERTY TO BE PART OF A PLAN FOR INFILL DEVELOPMENT?
3. IF "YES," DO YOU INTEND TO STAY IN YOUR PRESENT HOUSE?

NOT IN FAVOUR OF DEVELOPMENT

- SIGNED PETITION IN FAVOUR OF 50' LOTS

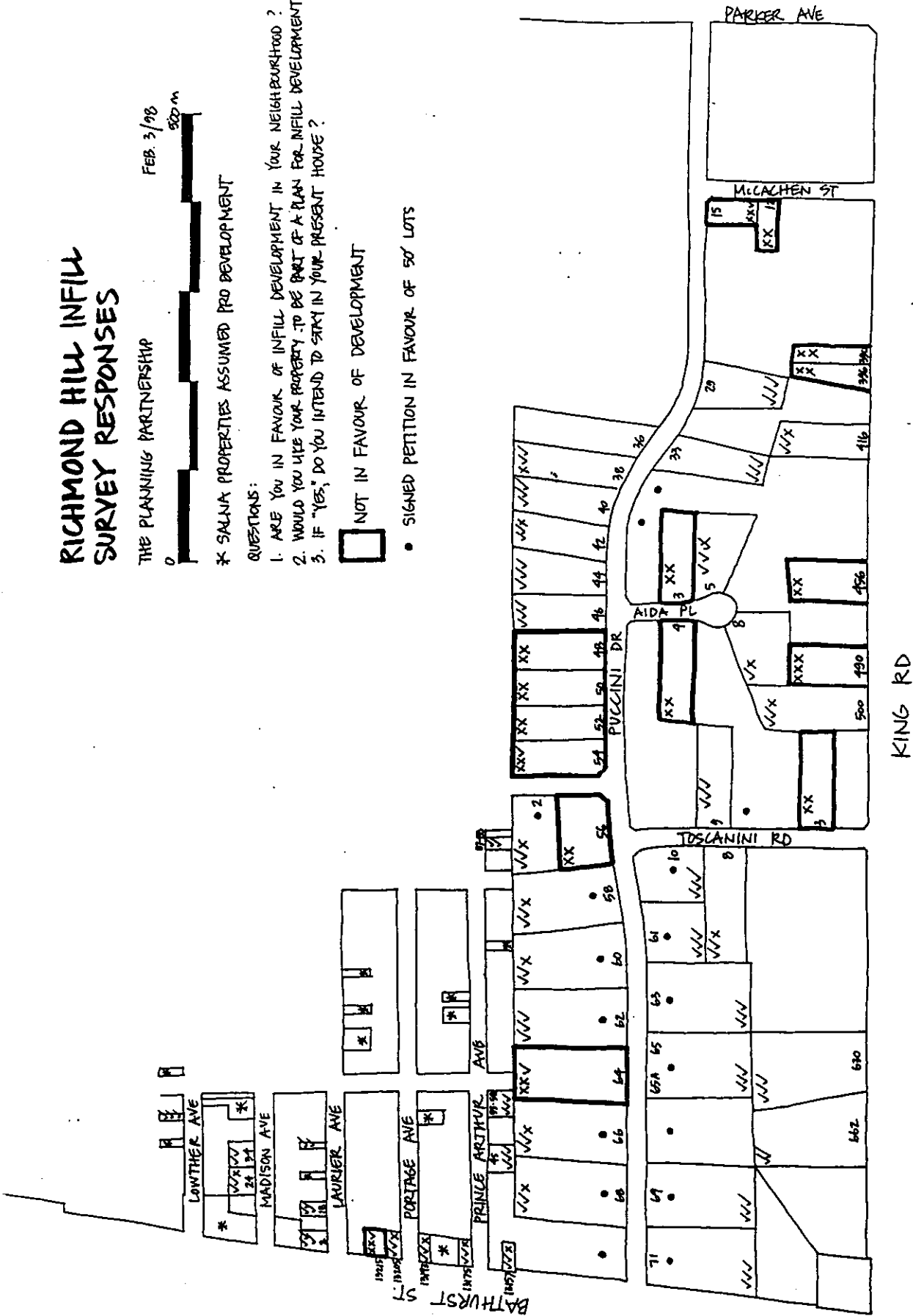


Figure 2

The discussion with Puccini residents focused on three issues:

- need to establish a minimum lot width on Puccini Drive to facilitate redevelopment;
- appropriateness of higher density residential and a mix of uses along Bathurst and the King Side Road; and,
- need for a minimum lot depth and lot width for rear lot development.

February Council presentation

A presentation was made to Council on February 4, 1998 for information on the options being explored for infill development in both the Puccini and Bathurst Street neighbourhoods. Following the Council presentation, Council was circulated with a petition signed by most residents on Puccini Drive west of Toscanini Road expressing a preference for 50 foot (15 m) lots. Those in favour of 50 foot (15 m) lots are indicated on Figure 2.

Third Workshop was held in April 1998

A final public workshop was conducted on April 18, 1998. The workshop was divided into three sessions: Bathurst neighbourhood, Puccini west (west of Verdi/Toscanini) and Puccini east. The purpose of the workshop was to gain consensus on the demonstration plans and guidelines described in the following chapters.

4.0 PUCCINI STREET NEIGHBOURHOOD DESIGN PRINCIPLES

The Puccini neighbourhood is characterized by small bungalows on large 2 acre lots, most of which were developed in the 1960's. The roads in the neighbourhood were developed in accordance with a rural standard with no curbs or sidewalks. Storm water is collected in roadside ditches. Mature trees are scattered throughout the neighbourhood. All existing development is now serviced by municipal water and sewer systems.

The character of the neighbourhood is largely the result of the house form, large front and sideyards and the rural road standard (Figure 3 a). Cross sections were prepared to analyse the difference among various frontages. With a 50 foot frontage (Figure 3b), the road will require reconstruction to a 20 m ROW urban street. There are no wide sideyards and houses will not be setback from the street. There were no noticeable changes in the character of the neighbourhood with an increase in frontage to 60 feet (18.3 m) (Figure 3c). Only when frontages were increased to 70 feet (21 m) (Figure 3d) was there wider sideyards and an opportunity for wider front yard setbacks, enhancing a more rural character of the neighbourhood. However, the appeal of 70 foot (21 m) wide lots

to builders, together with the lack of support from residents for larger lots, caused a consideration of the more widely accepted 50 foot (15 m) frontage.

A demonstration plan was prepared for the Puccini Street neighbourhood (Figure 4). It illustrates one scenario for development that achieves the general planning principles described below and the design guidelines set out in Chapter 6. The demonstration plan is not intended to serve as a final development concept and it is not intended to preclude concepts and design solutions that may be created as part of an actual development plan.

The demonstration plan illustrates 50 foot (15 m) frontage development on all properties with owners who petitioned Council, where ever site conditions permit. Wide shallow residential lots are illustrated on the rear lot areas accessed with new 60 foot (18.3 m) municipal roads. Loop road are suggested where ever possible. Drainage courses and areas with steep slopes have been protected in open space. Townhouses are illustrated in some locations along Bathurst Street and the King Side Road with rear access.

The key principles illustrated on the demonstration plan are summarized as follows:

Principle 1 The edges of the neighbourhood should be developed with higher density housing

The Bathurst Street and King Road frontages should be developed with higher density housing in the form of townhousing and walk-up apartments to serve as a buffer between the lower density housing and the major arterial roads. This form of housing is more feasible to develop since additional individual accesses to either road will not be granted by the Region of York.

Principles 2 The lot frontages on the existing streets should be no less than 15 metres

Although the vast majority of the residents on the existing streets strongly indicated that they wished to move and redevelop their lands, a minority wish to remain in the neighborhood. On this basis, it is recommended that the lot frontages be no less than 50 feet (15 metres) to assist in lessening the impact of developing more housing in the area on the existing neighbourhood. It is recognized that change in the neighborhood will take between 10 and 15 years, given the fragmented nature of the ownership in the area and the cost of developing land. As a result, the change in the character of the neighbourhood is expected to be gradual.

Principle 3 Traffic on Puccini Drive should be calmed

With new development in the area, traffic on Puccini Drive will increase and it will be used to by-pass the King Road/Bathurst Street intersection. As a result, traffic calming measures such as traffic circles and/or flow-through traffic restrictions should be considered.

Principle 4 *Natural features should be protected*

The existing watercourses in the area should be protected by ensuring that new development is sufficiently set back from the top of bank. Areas of significant slopes should also be protected from development.

5.0 BATHURST STREET NEIGHBOURHOOD DESIGN PRINCIPLES

The Bathurst Street neighbourhood consists of:

- the lands on the edge of the watercourse which cannot be developed and are proposed for storm water management and environmental protection in an approved draft plan. This plan provides very limited visibility and access to the valley;
- the lands which constitute the Draft Plan approval itself which are small lot singles, wide shallows, quads and townhouses, comprising a total of 226 units;
- the lands between the approved Draft Plan and the existing sparse development on the east side of Bathurst; and,
- existing undeveloped lands south of the watercourse.

These last 2 areas are the subject of infill considerations. A demonstration plan has been prepared for the Bathurst Street neighbourhood (Figure 5). It includes the draft plan that has already been approved with small lot singles, wide shallows, quads and townhouses, comprising a total of 226 units. The street pattern is extended, leaving only Madison and Portage open to Bathurst Street. Townhouses are illustrated along Bathurst Street as a buffer to the interior lower density residential neighbourhoods. A storm water management facility is illustrated with some additional open space to create a focus for the neighbourhood. The Town's engineering department is currently confirming the best location of stormwater management facilities.

The following are the key principles illustrated on the demonstration plan:

Principle 1 *There is a need to create a neighbourhood focus*

There is a greater need for some parkland in this intensely developed neighborhood, than is warranted in the Puccini Drive neighbourhood, especially because of limited access to the valleylands to the east. However, engineering considerations may make it

necessary to provide for storm water solutions behind the fringe development on Bathurst Street. This stormwater facility may be augmented with some additional usable space adjoining the facility to provide for a neighbourhood focus.

Principle 2 *There should be a transition in density*

The density of the infill site is proposed as a transition to the approved Draft Plan and the existing development. Any proposals for the Bathurst Street frontage would be similar to those proposed on the Bathurst frontage in the Puccini neighbourhood.

Principle 3 *Maintain the existing grid system of streets*

The development of the area between Lowther and the watercourse would require a limit of development to be established in consultation with the Conservation Authority. This has not been done and would be carried out by the future developer of these lands. We would assume that no further access is provided out to Bathurst Street for these lands, that this future development would be in a similar east west grid form, at similar densities to those approved in the Draft Plan, and that this development would be connected to the approved plan on the extensions of Verdi Road and Rideau Street. We would also expect that the street system would provide a much greater visibility and access to the valley.

6.0 DESIGN GUIDELINES

The infill study areas have been divided into the following four precincts depicting areas of similar existing conditions and opportunities for infill development (Figure 6):

1. King Side Road and Bathurst Street frontage
2. Puccini/Verdi/Toscanini frontage
3. Backlot areas
4. Bathurst Street neighbourhood

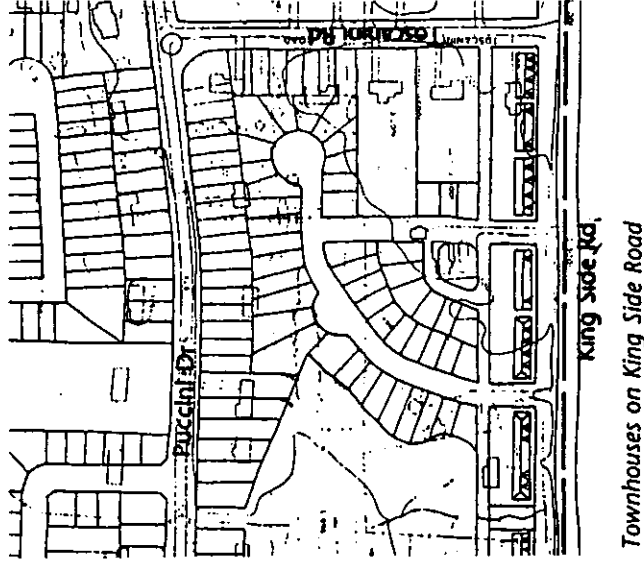
The Town will receive detailed concepts and draft plans for redevelopment of groups of residential properties. No scheme should be approved which imposes the continuation of a road system on adjoining properties which are not part of the submission. There should be an equal opportunity to select the form and character of redevelopment through the design process. The following guidelines should direct the detailed planning and design in each precinct.

a) King Side Road and Bathurst Street frontage

- A1 Residential uses should be predominantly medium density, ground-related housing.
- A2 Townhouses should be developed with a typical lot frontage of 20 feet (6 m).
- A3 No additional driveways from King Side Road and Bathurst Street will be permitted. New development is required to group access points or to consider shared access from rear lanes.
- A4 All development should be oriented to King Side Road and Bathurst Street.
- A5 Where new medium density development is proposed near existing low density development, measures to lessen the impact of new development should be considered. These measures include greater setbacks, landscape buffers and fencing.

b) Puccini/Verdi/Toscanini frontage

- P1 In order to accommodate redevelopment, Puccini, Verdi and Toscanini will be reconstructed to the Town's standard 66 foot (20 m) ROW with curbs, grass boulevard and sidewalks.
- P2 Existing trees should be preserved where ever possible.



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P3 Deciduous trees should be planted along the street edge to form a continuous canopy at maturity.

P4 Poles, lights, signs, transformers and other services should be located along the street tree planting line wherever feasible, to minimize clutter and disruption of the street's character.

P5 Utilities, including transformers and other "box" servicing should be located unobtrusively in areas where they can be screened with planting, in flankage locations or within the boulevard.

P6 The minimum frontage of residential lots on Puccini, Verdi and Toscanini shall be 50 feet (15 m) with a 5 foot (1.5 m) sideyard setback.

P7 The front yard setback shall be 15 feet (4.5 m) to the main wall of dwelling.

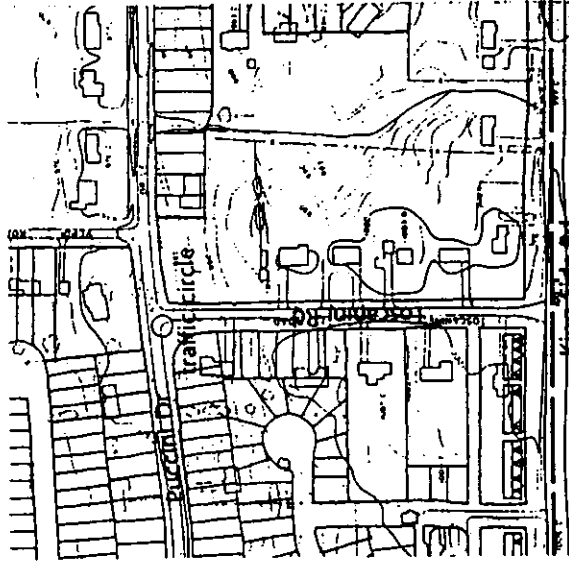
P8 Buildings shall have front walls parallel to the street with front doors, windows and porches on the street to create a consistent street wall;

P9 New houses should be oriented to existing streets on flankage streets. The main entrance should be on the existing street, not the new flankage street.

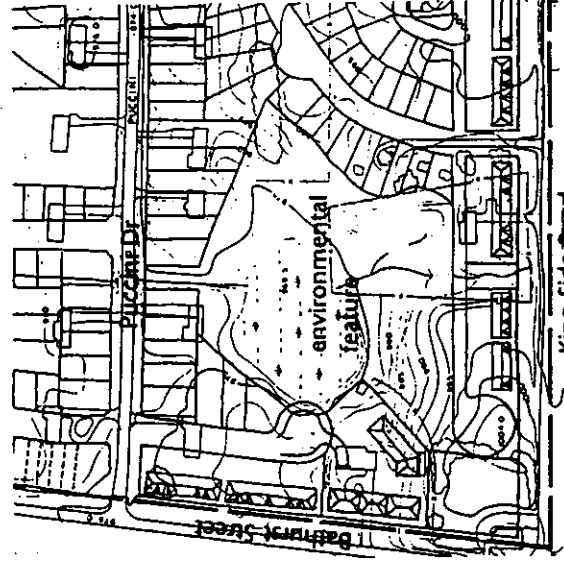
P10 The front face of the garage shall not extend beyond the front face of the living area.

P11 Urban open space/roundabouts could be used to calm traffic on Puccini Drive and direct traffic flows without necessarily requiring stop signs at intersections and, above all, to create a space or "sense of place" out of what would otherwise simply be the intersection of two roads. It is the intention that whenever these elements are used they should be significant landscape features, as well as traffic controls.

P12 Environmental features including steep slopes, poorly drained areas, water courses and vegetation, shall be protected in redevelopment plans.



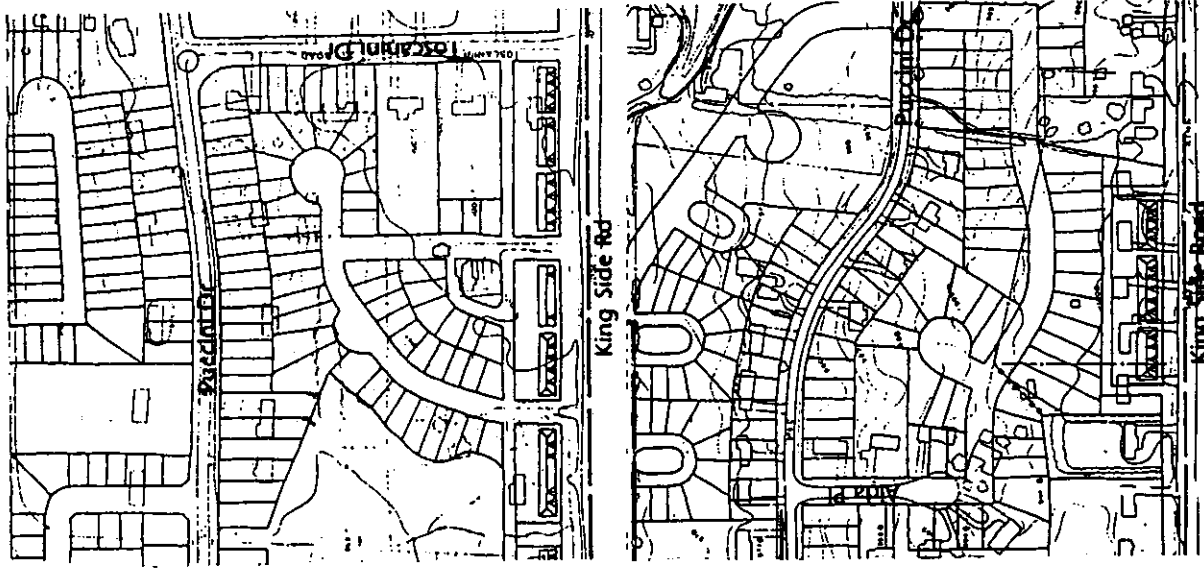
Traffic calming circle on Puccini Drive



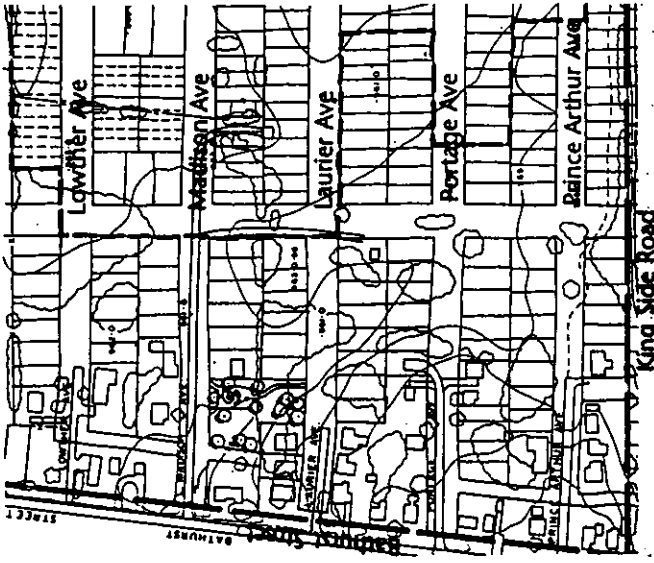
Environmental features protected in concept plan

000024

- c) **Rear lot areas**
- R1 In order to accommodate redevelopment, in the rear lot areas, new roads shall have a 60-66 foot (18-20m) ROW.
- R2 Houses should face both sides of the street.
- R3 New roads should have a variety of deciduous street trees planted 26 - 32 feet (8 to 10 m) on-centre.
- R4 Poles, lights, signs, transformers and other services should be located along the street tree planting line wherever feasible, to minimize clutter and disruption of the street's character.
- R5 Utilities, including transformers and other "box" servicing should be located unobtrusively in areas where they can be screened with planting, in flankage locations, in open space, laneways and on private property or within the boulevard.
- R6 Shallow wide lots with a 82 feet (25 m) depth and 40 foot (12 m) frontage are acceptable.
- R7 In no case, for wide shallow lots, shall the front face of the garage extend beyond the front face of the living area.



Shallow wide lots on 18.5m roads in rear lots



Road grid maintained in infill development

- d) Bathurst Street neighbourhood**
- B1** The road grid should be maintained.
- B2** Minimum lot width can be 35 to 40 feet (10.6 - 12m) in this area due to the sparse nature of the existing development. Larger lot widths could be considered adjacent to existing developed lots.
- B3** Buildings shall have front walls parallel to the street with front doors, windows and porches on the street to create a consistent street wall;
- B4** Reduced setbacks should be considered with 10 feet (3m) to the front face of the living area and 20 feet (6m) to the garage.
- B5** Porches stairs, canopies and other entrance features shall be permitted in the front yard setback;
- B6** If the location for the storm water management pond is changed, then alternative municipally owned lands central to the neighbourhood are suggested for parkland purposes.
- B7** The stormwater management pond should be designed as a significant and integral feature of the landscape.
- B8** The grading of the stormwater management pond should ensure natural and variable side slopes and sinuous contours.
- B9** Planting around the stormwater management pond should be comprised of native species and flood tolerant water's edge plants to stabilize banks.

7.0 ZONING PROGRAM

7.1 King Side Road and Bathurst Street frontage

These lands would be placed in a site-specific RM1 Zone. All of the provision of By-law 313-96 that deal with RM1 Zone are appropriate, except that:

1. Parking areas and individual driveways should not be permitted in the yard abutting either King Side Road or Bathurst Street.
2. No less than 70% of the main wall facing King Side Road or Bathurst Street shall be located any farther than 20 feet (6m) from the edge of the ROW.

Site-specific zoning at intersections to permit non-residential development and mixed use would be implemented as the need arises. The provisions of the RM1 Zone are attached.

7.2 Puccini/Verdi/Toscanini frontage

These lands would be placed in a site-specific R8 Zone. All of the provision applying to the R8 Zone in By-law 313-96 are appropriate. There should also be provision that states that no driveway shall cross an exterior side lot line and that any lot line abutting Puccini is deemed to be the front lot line for the purpose of this By-law.

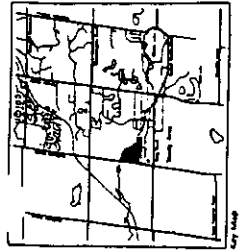
7.3 Backlot areas

These lands would be placed in a site-specific RWS1 Zone. All of the provision applying to the RWS1 Zone in By-law 313-96 are appropriate, except that:

1. The minimum lot frontage on interior lots should be decreased to 40 feet (12 m).
2. The minimum lot frontage on corner lots should be decreased to 43 feet (13.2 m) and in no case shall the front of the garage extend closer to the street than the front of the living area or porch.

7.4 Bathurst Street neighbourhood

These lands would be placed in the R3 Zone. No changes to the R3 Zone standards are required. These standards are attached.

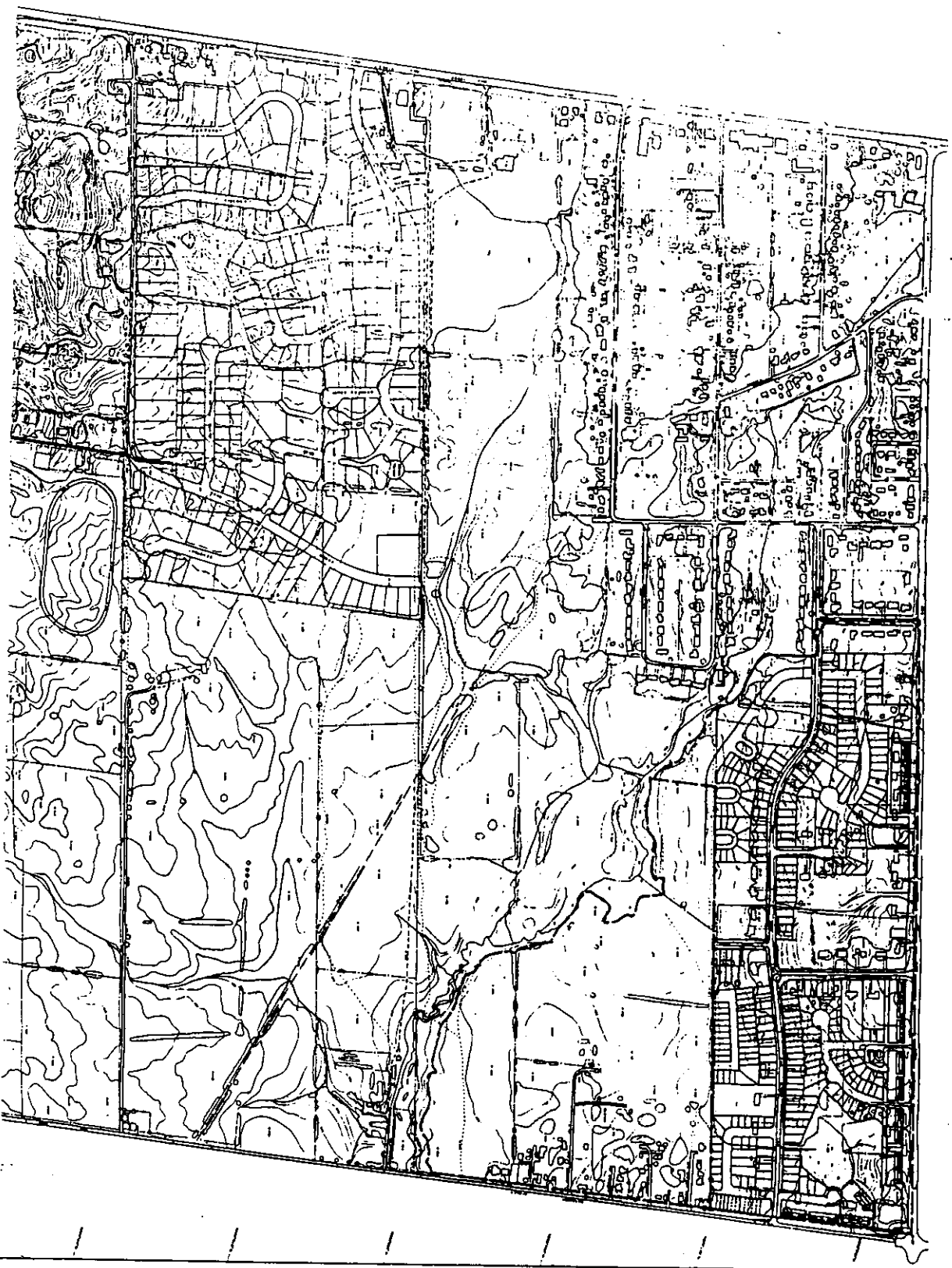


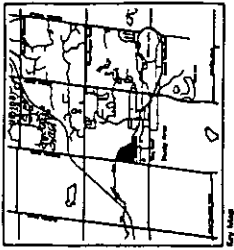
Demonstration Plan
Area 1
Puccini West

Town of Richmond Hill

Residential Infill
Bathurst Street
Neighbourhood

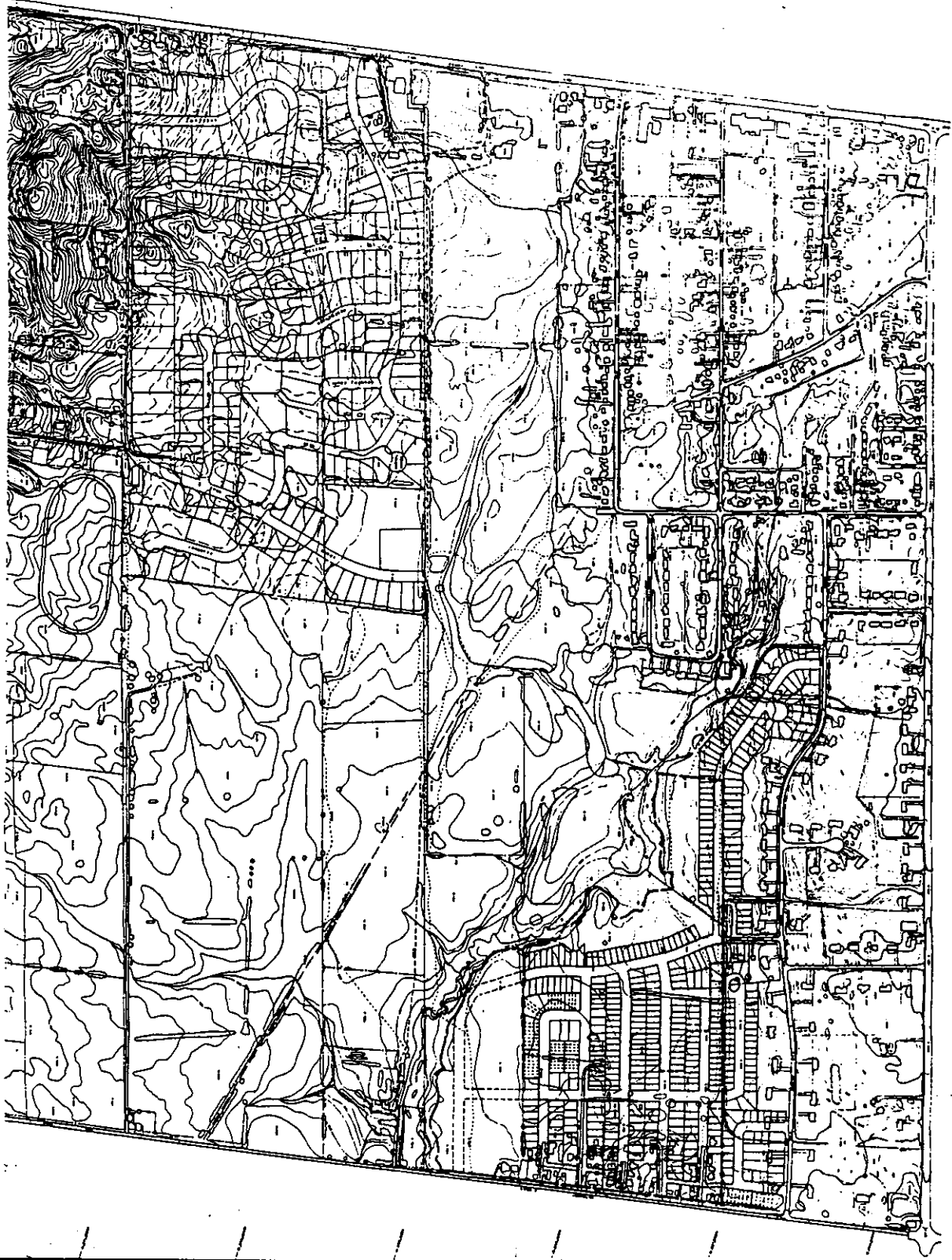
The Planning Partnership





Town of Richmond Hill
Residential Infill
Bathurst Street
Neighbourhood

The Planning Department

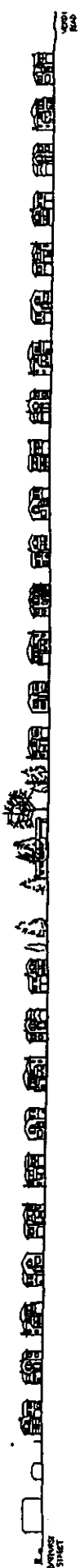




3a ELEVATION
NORTH SIDE OF PUCCINI DR.
EXISTING



3b ELEVATION
NORTH SIDE OF PUCCINI DR.
75' FRONTAGES



3c ELEVATION
NORTH SIDE OF PUCCINI DR.
60' FRONTAGES



3d ELEVATION
NORTH SIDE OF PUCCINI DR.
50' FRONTAGES

Figure 3

APPENDIX "B"

DESIGN GUIDELINES FOR THE PUCCINI DRIVE NEIGHBOURHOOD

The following guidelines should guide the detailed planning and design in the Puccini Drive neighbourhood:

- a) King Side Road and Bathurst Street frontage
 - A1 Medium density residential development is encouraged on King Side Road and Bathurst Street.
 - A2 Medium density is to be ground-related housing.
 - A3 Townhouses should be developed with a 6 metre frontage.
 - A4 No additional driveways from King Side Road and Bathurst Street will be permitted. New development is required to group access points or to consider shared access from rear lanes.
 - A5 All development should be oriented to King Side Road and Bathurst Street.
 - A6 Where new medium density development is proposed near existing low density development, measures to lessen the impact of new development are to be considered. These measures include greater setbacks, buffers and fencing.

- b) Puccini/Verdi/Toscanini frontage
 - P1 In order to accommodate redevelopment, Puccini, Verdi and Toscanini will be reconstructed to the Town's standard 20 metre ROW with curbs, grass boulevard and sidewalks.
 - P2 Existing trees should be preserved where ever possible.
 - P3 Deciduous trees should be planted along the street edge to form a continuous canopy at maturity.
 - P4 Poles, lights, signs, transformers and other services should be located along the street tree planting line wherever feasible, to minimize clutter and disruption of the street's character.
 - P5 Utilities, including transformers and other "box" servicing should be located unobtrusively in areas where they can be screened with planting, in flankage locations or within the boulevard.

- P6 The minimum frontage on Puccini, Verdi and Toscanini shall be 50 feet (15 metres) with a 5 foot (1.5 metres) sideyard setback.
- P7 The front yard setback shall be 15 feet (4.5 metres).
- P8 New houses should be oriented to existing streets on flankage streets. This means that the main entrance should be on the existing street, not the new flankage street.
- P9 An urban open space/roundabout could be used to calm traffic on Puccini Drive and direct traffic flows without necessarily requiring stop signs at intersections and, above all, to create a space or "sense of place" out of what would otherwise simply be the intersection of two roads. It is the intention that whenever these elements are used they should be significant landscape features, as well as traffic controls.
- P10 Environmental features including steep slopes, poorly drained areas, water courses and vegetation, shall be protected in redevelopment plans.

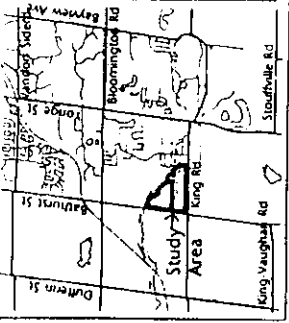
c) Rear lot areas

- R1 In order to accommodate redevelopment, in the rear lot areas, new roads shall have an 18 to 20 metre ROW.
- R2 New roads should have a variety of deciduous street trees planted 26 - 32 feet (8 to 10 metres) on-centre.
- R3 Poles, lights, signs, transformers and other services should be located along the street tree planting line wherever feasible, to minimize clutter and disruption of the street's character.
- R4 Utilities, including transformers and other "box" servicing should be located unobtrusively in areas where they can be screened with planting, in flankage locations, in opens pace, laneways and on private property or within the boulevard.
- R6 Shallow wide lots with a 82 feet (25 metres) depth and 40 foot (12 metre) frontage area acceptable.
- R7 In no case, shall the front face of the garage extend beyond the front face of the living area.

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Demonstration Plan Puccini Neighbourhood

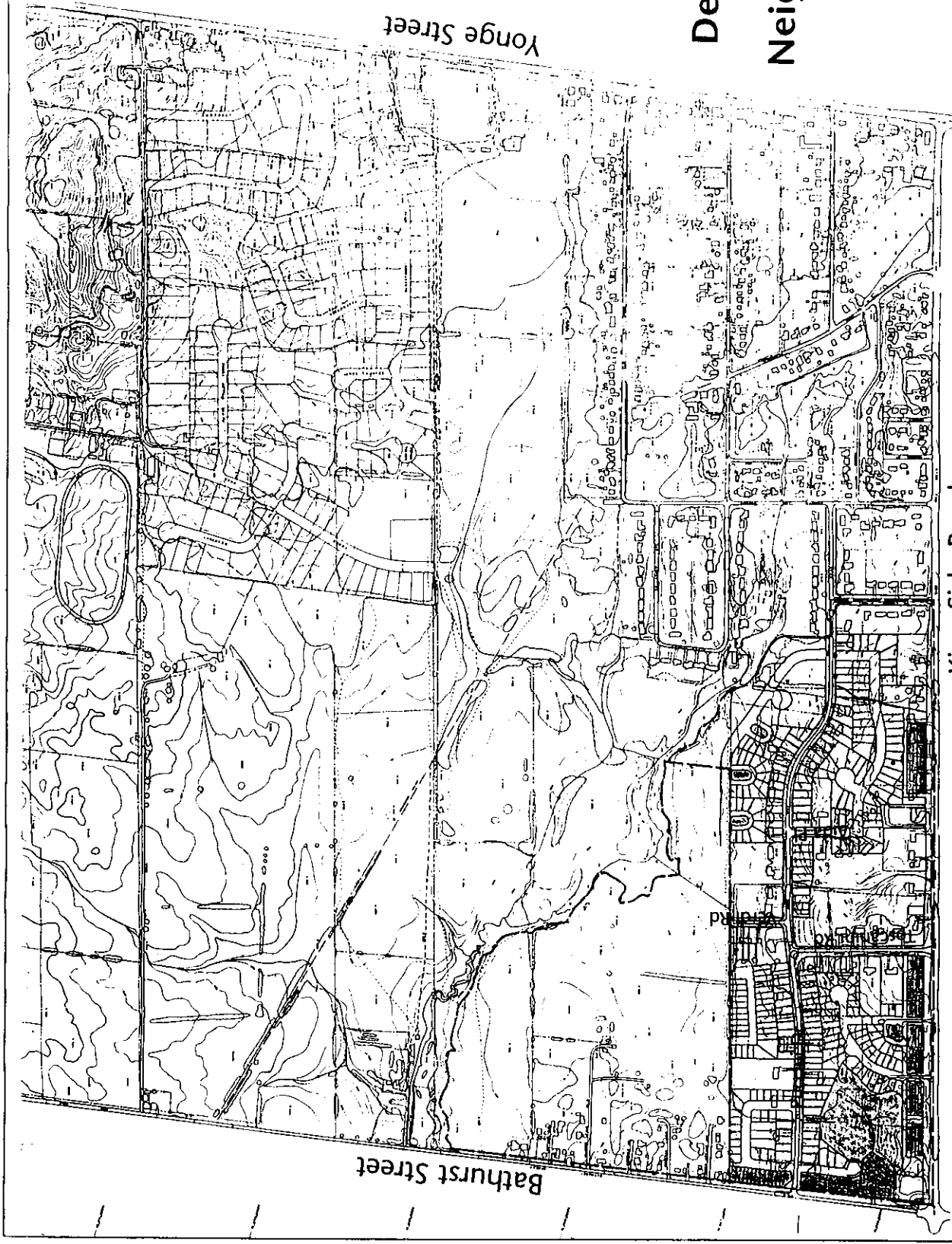
Figure 4



Key Map

LEGEND

- low density residential
- medium density residential
- open space



King Side Road

TOWN OF RICHMOND HILL
PLANNING AND DEVELOPMENT
DEPARTMENT

JUN 02 1998

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